



# CABINET

THURSDAY, 28TH JANUARY, 2016

At 7.30 pm

in the

COUNCIL CHAMBER - TOWN HALL,

## SUPPLEMENTARY AGENDA

### PART I

<u>ITEM</u>	<u>SUBJECT</u>	<u>PAGE NO</u>
iii.	Windsor Office Accommodation Update	3 - 118

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# hunters

## Feasibility report for the joint venture office and police station for

ROYAL BOROUGH OF WINDSOR & MAIDENHEAD

and

THAMES VALLEY POLICE

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Final draft 18.12.14





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This report has been prepared by Hunters Architects on behalf of Thames Valley Police and the Royal Borough of Windsor & Maidenhead.

The consultant team advising on this report is as follows:

Architect:	<b>Hunters</b>
Planning Consultant:	<b>GL Hearn</b>
Structural Engineer:	<b>Tully DeAth</b>
Services Engineer:	<b>AWA</b>
Sustainability Consultant:	<b>XCO<sup>2</sup></b>

The Royal Borough of Windsor and Maidenhead (RBWM) and Thames Valley Police (TVP) both own properties within Windsor which are now considered to be largely not “fit for purpose”. Following detailed options analysis carried out separately by both parties it has been agreed that potentially a joint venture would provide the best method of delivering the level of service required to local communities and reduce on-going revenue costs.

Thames Valley Police’s existing police station, located in Alma Road Windsor, was a purpose built station constructed in 1972. It consists of a main block with 5 levels with a gross internal floor area of 3,070m<sup>2</sup>. In addition there are various garaging and outbuildings in a secure yard as well as on site parking, with an overall site area of 1,304m<sup>2</sup>.

The existing building requires a significant amount of essential maintenance and infrastructure work to be undertaken to bring the facility back up to a reasonable standard. It is also significantly larger than required with the current (and planned) level of staffing.

In order to reduce its commitments and ongoing revenue costs, Thames Valley Police are now investigating the opportunities to “downsize” into more appropriately sized accommodation.

The Royal Borough of Windsor and Maidenhead (RBWM) have an existing office complex with public access counter (York House) located in Sheet Street, Windsor. This office complex was constructed in the 1970’s, with a later extension at the rear constructed in circa 1996/97.

The complex includes an element of basement parking as well as external parking which is dedicated for office use during weekdays but is used as additional public parking at weekends and after 6pm on weekdays. The car park provides 92 parking spaces and is operated as a pay & display car park when open to the public.

Sheet Street is a busy road leading south from Windsor Town Centre. Army barracks are located opposite the property with part of Windsor Great Park to the rear. Immediately to the north of York House is a detached office building with a good quality flat development behind. There is a terrace of attractive period houses to the south.

The existing premises are inefficiently laid out and also require a significant amount of essential maintenance and infrastructure work to be undertaken to bring the facility back up to a reasonable standard. A recent review of staffing levels and working practices has also identified that the existing accommodation is larger than current requirements.

In order to try and resolve the requirements of both parties, it has been agreed by both the Police and Crime Commissioner for Thames Valley and also the Royal Borough of Windsor and Maidenhead Council that the opportunity for a possible joint development is examined fully for financial and practical consideration.

The initial phase is to undertake sufficient feasibility studies in order to establish if there is sufficient common ground to proceed with a joint redevelopment of one of the sites.

This report provides a high level appraisal of each of the potential sites in order to determine whether or not a joint facility is possible and recommends a way forward for design development. This report does not provide a complete design solution for the proposed joint venture.

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The brief for this feasibility report has been to review several development sites within Windsor to determine if they are suitable for the redevelopment of both a new council office and a new police station for the town.

Both organisations require a "town centre" location that is accessible on foot from the centre of Windsor. Thames Valley Police will be operating local police services from the new facility and it will also have a public facing counter for public enquiries as well as bail reporting and acting as a base for operational and responsive police officers. It is felt that a town centre presence is also important for residents within the town. For the purposes of this report, the centre of town is defined as the junction of Peascod Street and Thames Street.

RBWM also require a central location within the town to enable easy access for residents to call in as well as providing office facilities for various council departments.

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Discussions are ongoing between RBWM & TVP on the viability of a totally shared facility.

Both RBWM and TVP have specific requirements for their buildings which are set out over the next few pages.



Centre of Town, Windsor

### Thames Valley Police Requirements

The very first option considered by Thames Valley Police was to do nothing and retain their existing office on Alma Road. This has been dismissed on the grounds of future costs and value for money.

Due to the sensitive nature of Police operations and the confidentiality of information displayed and discussed within the police station, it is felt that a stand alone building is required. Parking and other communal facilities could be shared with RBWM but operational meeting rooms and offices must be segregated and specifically for TVP use.

The preliminary outline requirements are as follows:

1. Patrol and neighbourhood operational policing base of approximately 550m2 GFA – ideally located on 2 floors
2. No custody requirements
3. Front counter facility with associated office and interview room and property store of approximately 70m2 GFA
4. Public access to front counter
5. Secure parking area for 20 marked vehicles including large van and 25 cycles in secure store
6. Additional staff and public parking of 35 vehicles
7. Energy efficiency target of 'A' rating Naturally ventilated offices.



Existing TVP office, Alma Road, from north



Existing TVP office, Alma Road, from south

### Royal Borough of Windsor & Maidenhead Requirements

As the only public facing council facility within the town it is felt that the architectural design and approach should be excellent. A town centre location is essential. Some facilities should be made available to the wider public (such as meeting rooms etc) as well as ward councillors and opposition members.

Due to the nature of some operations within this building, "vulnerable" people may be required to visit site. As such, a shared office or reception facility with TVP is considered inappropriate.

The preliminary outline requirements are as follows:

1. Modern flexible largely open plan office space of approximately 1250 m<sup>2</sup> GFA (12,000 sq ft NIA) – approximately 170 – 180 modern workstations.
2. Possibility for additional space for 3rd party collaborative occupancy.
3. Public access front counter Meeting rooms – a mixture of large (10 – 15 people); smaller (6-8 people) and focus rooms (2-4 people).
4. Energy efficiency target of 'A' rating
5. Parking for staff and public visitors of 100 vehicles.



Existing RBWM office, York House, Sheet Street from south



Existing RBWM office, York House, Sheet Street from north

Discussions have been ongoing between TVP and RBWM officers to develop the specific design requirements for each facility between May and September 2014. The overall size of the RBWM office has increased from the preliminary brief during these discussions to explore the potential for relocation of staff from the Tinkers Lane facility.

Additional capacity has been created to accommodate an increase in staffing numbers by 10% to allow for contingency growth. Additional spaces, such as trading standards laboratory, first aid station etc have also been added following detailed discussions with RBWM operatives.

The following pages set out the detailed brief requirements for both parties. Requested m<sup>2</sup> have been tested and confirmed in the following tables.

The areas contained within these schedules have been used to test the initial viability of each site.

The room data sheets within this report represent **POSSIBLE** spacial arrangements to test the viability of any option and are not intended as

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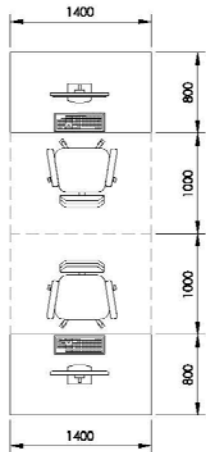


Reception & Waiting  
(80m<sup>2</sup>)

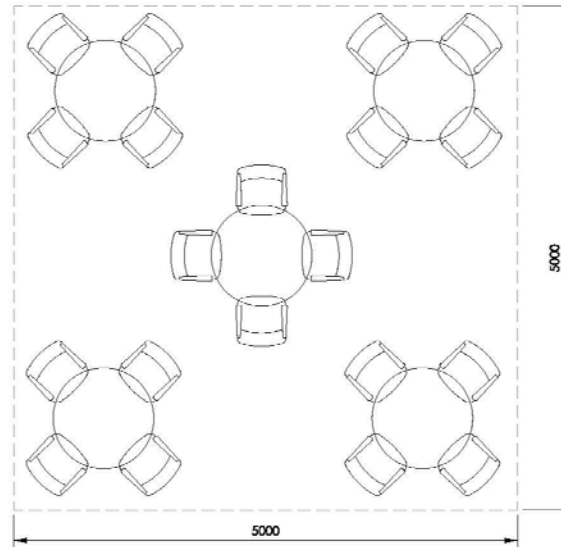
## Royal Borough of Windsor & Maidenhead

Name	Number	Area m <sup>2</sup>	Total m <sup>2</sup>	
Workstations				
Resident Workstation	163	7	1141	based upon 227 workstations @ 0.65% occupancy (plus 10%)
<b>Open Plan</b>				
Tea Point (Kitchenette)	3	7.5	22.5	assume 1 per floor (subject to final layout)
Break-out space	3	25	75	assume 1 per floor (subject to final layout)
Print station	3	10	30	based upon 1 per 60 people
<b>Meeting Rooms</b>				
Large Meeting Room (12 person)	4	20	80	
Small Meeting Room (6 person)	8	10	80	
<b>Reception</b>				
Reception Area	1	50	50	
Waiting Area	1	30	30	
Private Meeting Rooms (4 people)	3	9	27	
Public Conference Room (20 people)	1	49	49	
Members Room (10 people)	1	20	20	
Opposition Room (10 People)	1	20	20	
Union Room (6 people)	1	10	10	
Kitchenette (to serve meeting rooms)	1	5	5	
Locker/cloaks room	1	6	6	
trolley store	1	3	3	
<b>Ancillary</b>				
Medical station	1	6	6	
Staff Lockers	62	1.5	93	based upon 227 staff plus 10% stacked 4 high
Shower Room/Changing	2	5	10	
Drying Area	1	6	6	
IT/Server Room	1	21	21	
Refuse / Recycling (office)	9	3	27	based upon 1 per 20 people
Refuse store / Recycling (collection)	1	50	50	Central collection bin store (assume weekly)
WC	6	9.5	57	based upon 1 x male & female per floor
Disabled WC	3	4.5	13.5	based upon 1 per floor
Trading standards evidence store	1	6	6	
Trading standards laboratory	1	6	6	
Café	1	128	128	for 80 people - 50% occupancy (1.6m <sup>2</sup> per person)
Commercial kitchen	1	40	40	based upon 0.5m <sup>2</sup> per person
			<b>NIA 2112</b>	circulation (say 15%) 316.8
			<b>GIA 2428.8</b>	
Public Parking Space	100	(2.4m x 4.8m)		

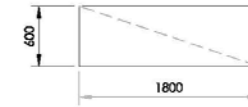
Indicative Room Layouts



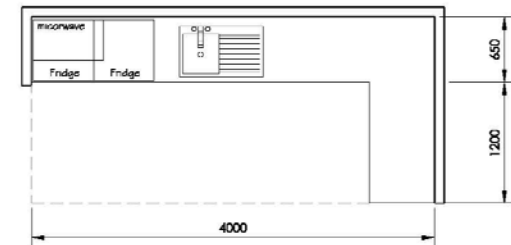
Workstations



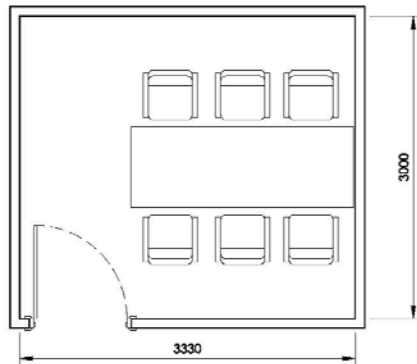
Break-out space (25m<sup>2</sup>)  
informal meeting



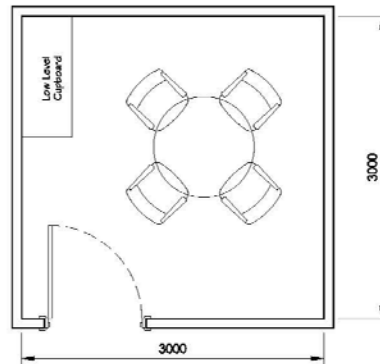
Personal lockers  
(1 m<sup>2</sup>)



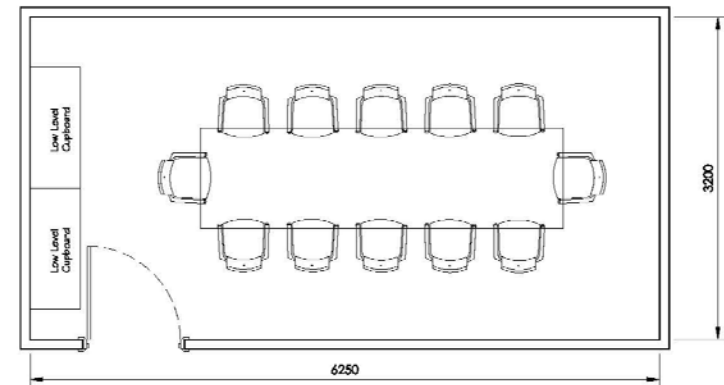
Tea Point (7.5m<sup>2</sup>)



Small Meeting Room  
(10m<sup>2</sup>)



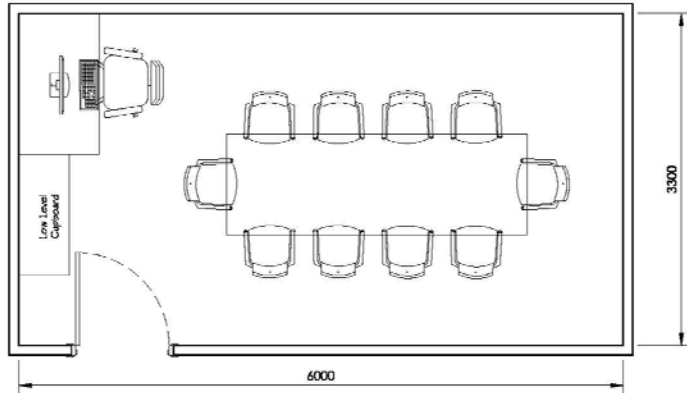
Private Meeting Room  
(9m<sup>2</sup>)



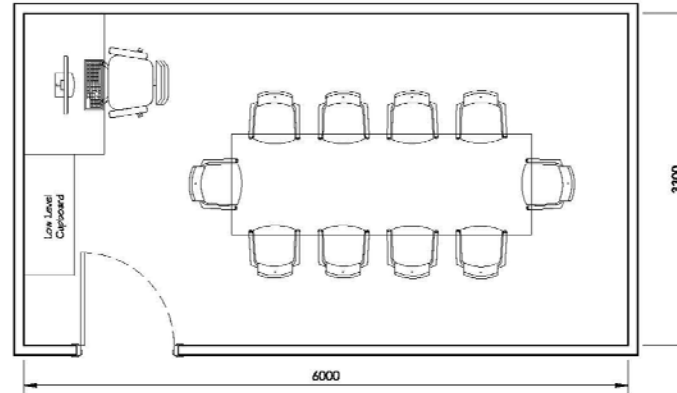
Large 12 Person Meeting Room  
(20m<sup>2</sup>)

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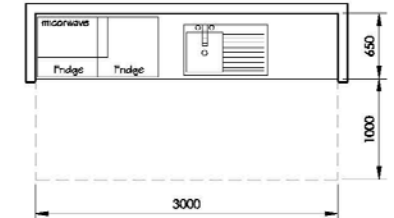
Indicative Room Layouts



Members Room (20m<sup>2</sup>)

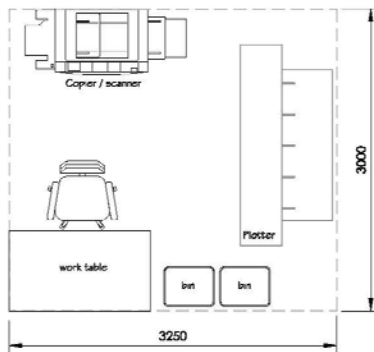


Opposition Room (20m<sup>2</sup>)

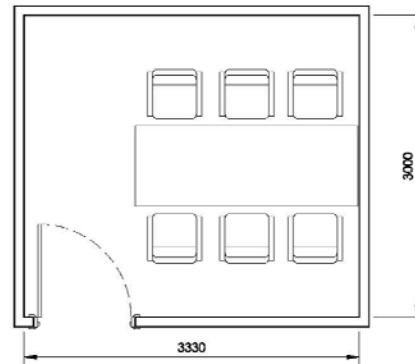


Meeting Room Tea Point (5m<sup>2</sup>)

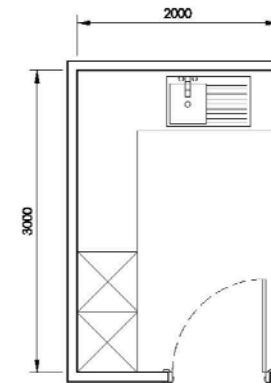
13



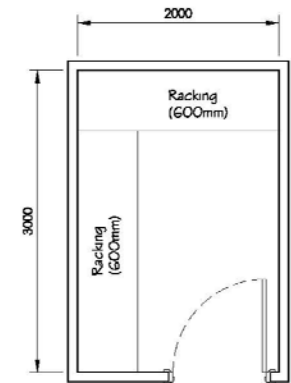
Print station (10m<sup>2</sup>)



Union Room (10m<sup>2</sup>)

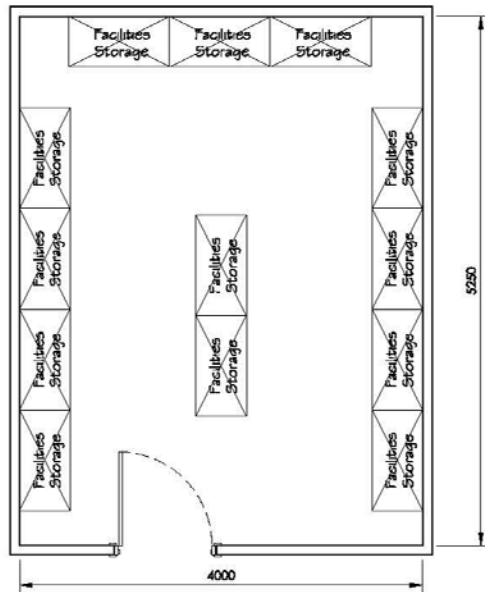


Trading standards lab (6m<sup>2</sup>)

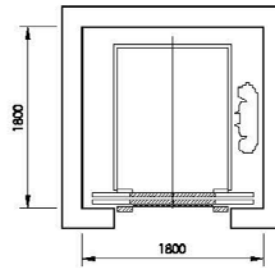


Evidence Store (6m<sup>2</sup>)

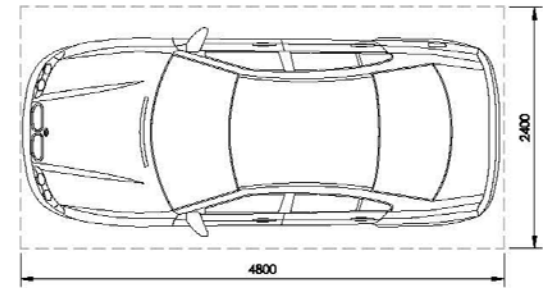
Indicative Room Layouts



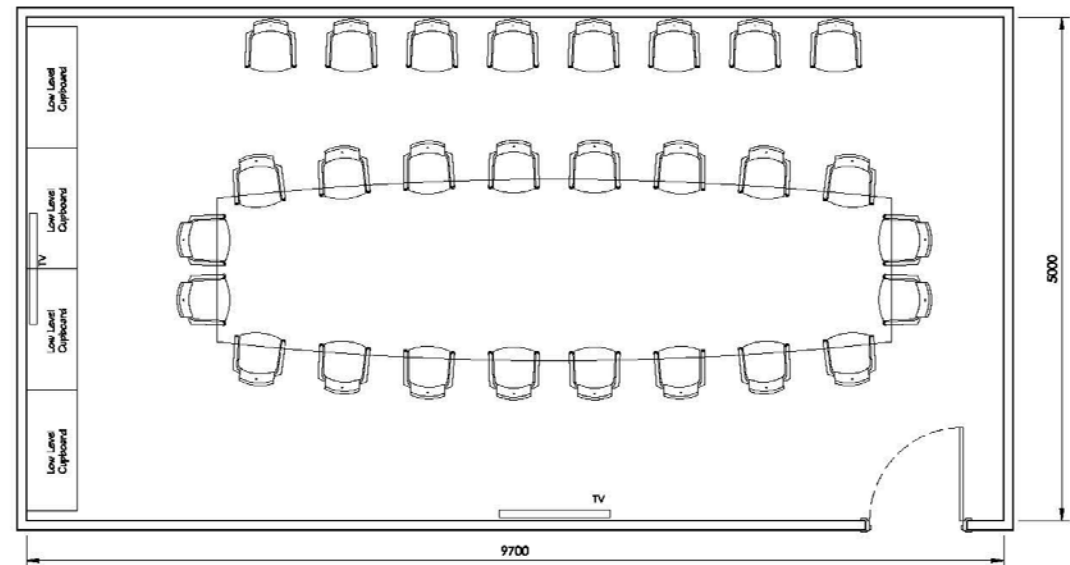
ICT / Comms room (21m<sup>2</sup>)



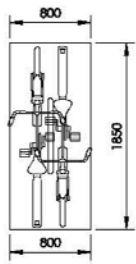
Lift (3.25m<sup>2</sup>)



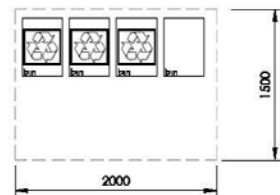
Car Parking Bay



Public Conference Room (49m<sup>2</sup>)



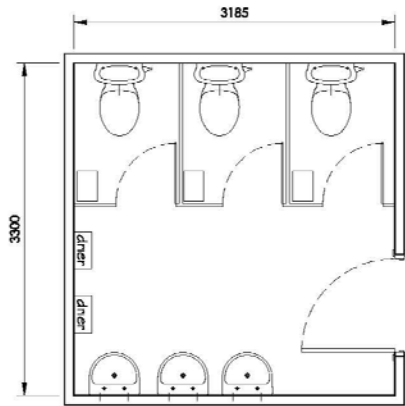
Cycle Parking x2



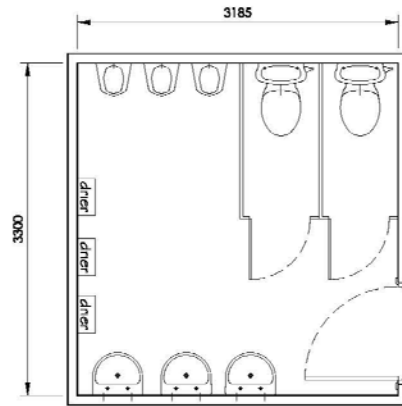
refuse (3m<sup>2</sup>)

14

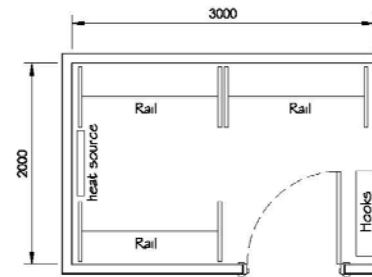
Indicative Room Layouts



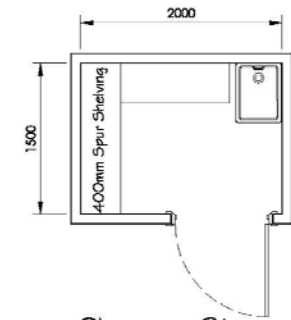
Female WC (9.5m<sup>2</sup>)



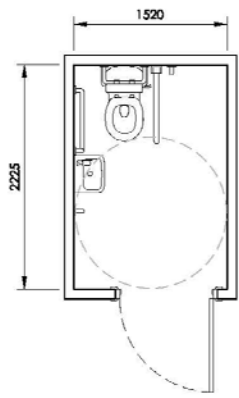
Male WC (9.5m<sup>2</sup>)



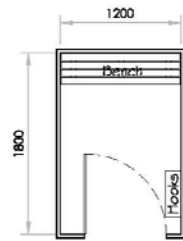
Drying Room (6m<sup>2</sup>)



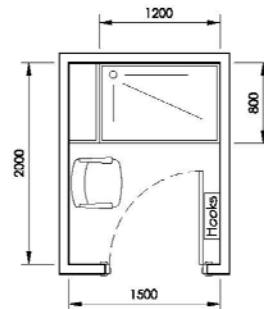
Cleaners Store (3.0m<sup>2</sup>)



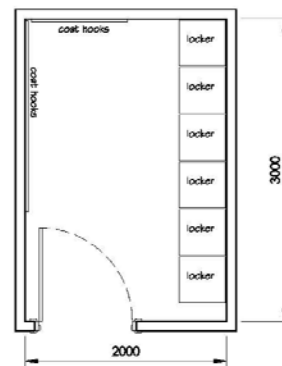
Disabled WC (2.5m<sup>2</sup>)



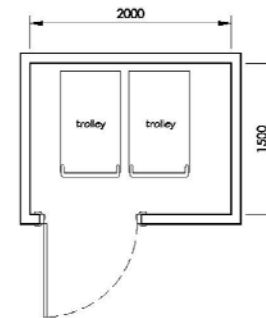
Changing (2m<sup>2</sup>)



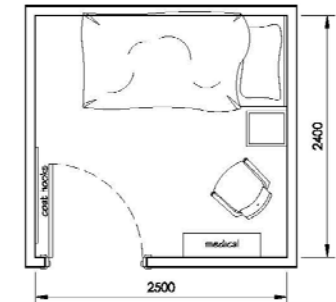
Shower Room (3m<sup>2</sup>)



Locker Room (6m<sup>2</sup>)



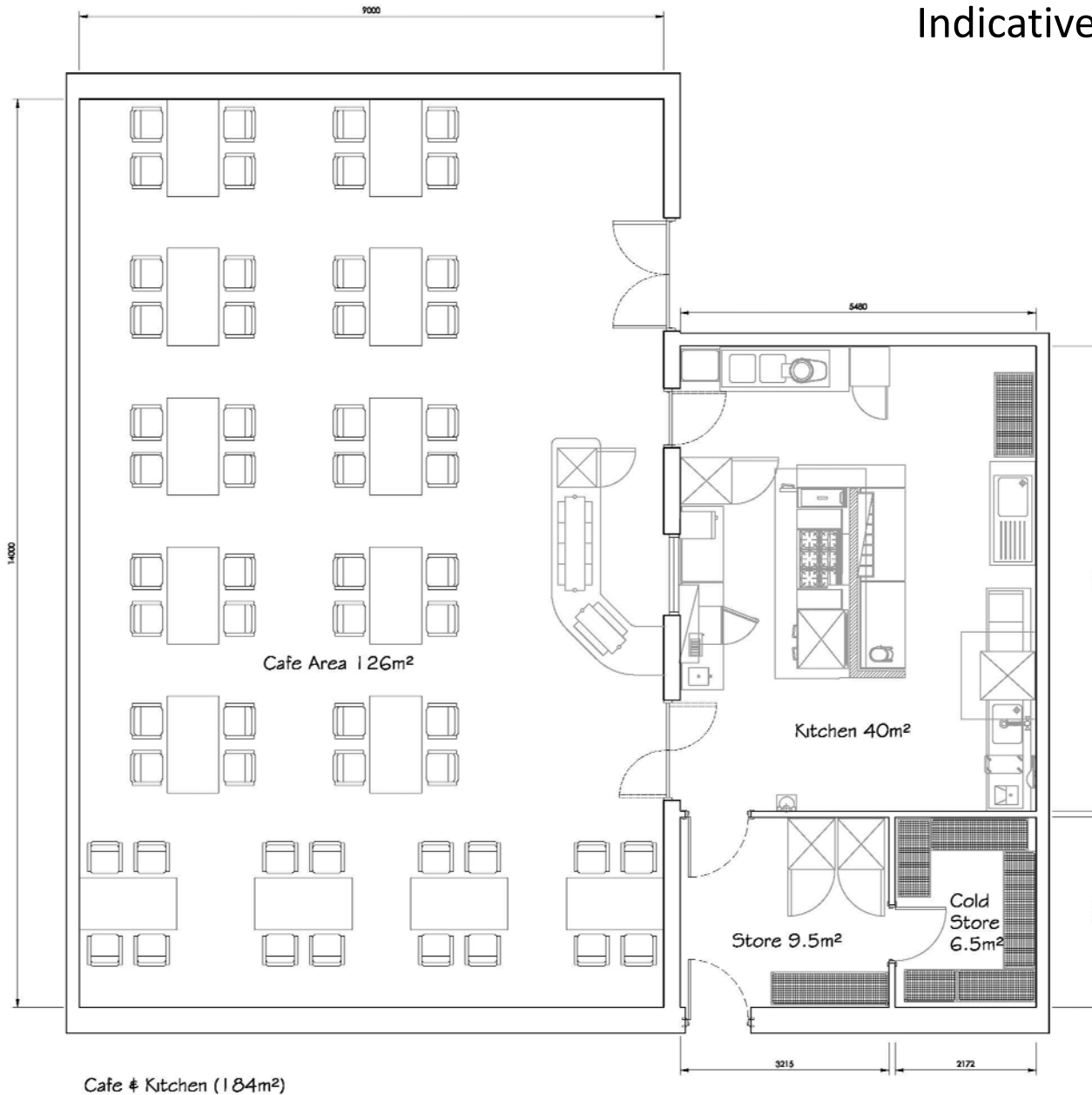
Trolley store (3m<sup>2</sup>)



Medical Room (6m<sup>2</sup>)

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Indicative Room Layouts



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During discussions with Thames Valley Police the overall GIA of the proposed police station has increased from the original brief. This has been due to the specific requirements of a police station in this location being addressed and additional facilities being required.

Two Options have been developed:

**Option 1**- 2 storey building with reduced size "Silver Suite" and limited potential for expanding building occupancy.

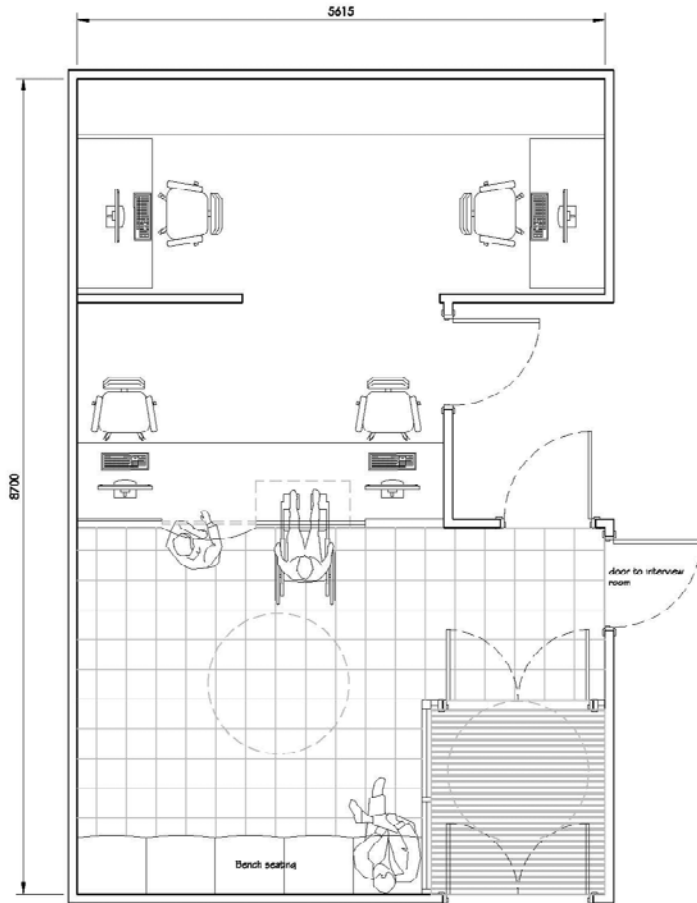
**Option 2** – 3 storey building with increased "Silver Suite" and capacity for additional personnel.

The areas represented in these schedules provide a guide and have been used to assess the suitability of the individual sites. The final buildings will vary depending on site layout and footprint of the external envelope.

### Thames Valley Police – OPTION 1 (2 storeys)

Name	Number	Area m <sup>2</sup>	Total m <sup>2</sup>		
<b>Workstations</b>					
Resident Workstation	4	2.5	10		
Ops highly Mobile Workstation	8	2.5	20		
<b>Open Plan</b>					
Touchdown Workstation	2	1.6	3.2		
Personal storage	32	1	32		
Break Out Space (informal meeting)	1	6	6		
<b>Cellular Office</b>					
Office Type 2	1	9	9		
<b>Ancillary Office</b>					
Study Booth	1	4.5	4.5		
<b>Ancillary</b>					
Locker room (102 lockers)	1	53	53		
Filing Room (records)	1	20	20		
8 -10 person Meeting Room	1	20	20		
Voluntary Interview Room	1	10	10		
Tea Point (kitchenette)	1	6	6		
Print / Photo Copy Station	1	2.4	2.4		
<b>Support</b>					
Silver Suite (12-18 person meeting room)	1	36	36		
Visitor Touchdown (in reception)	2	1.6	3.2		
ICT Comms. Room	1	9	9		
CCTV Server Room	1	5	5		
Lap top charging point	1	2	2		
Faith Room	1	9	9		
Reception & SDO Type 2	1	50	50		
Property / Evidence Store	1	15	15		
Changing Cubicle (in locker room)	1	3	3		
Shower Room	2	3	6		
Drying Room	1	6	6		
Cleaners Store	1	3	3		
WC	5	5	25	2 x male, 2 x female, 1 x disabled	
<b>Storage</b>					
Cone Store	1	10	10		
Taser Store	1	6	6		
Bodyworn Video & CCTV Review	1	9	9		
Post Room	1	2	2		
PSU Bag / Equipment Store	1	15	15		
Trap Bikes	1	9	9		
Leaflet Store	1	3	3		
			<b>NIA 422.3</b>		
			<b>GIA 485.6</b>		
				circulation (say 15%)	63.3
Secure Car Parking Space	19	(2.8m x 4.8m)			
Van Parking Space	1	(3.0m x 5.8m)			
Public Parking Space	35	(2.4m x 4.8m)			
Cycle Parking	35	(0.8m x 1.85m)			

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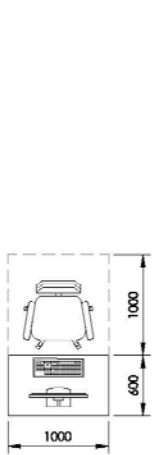


Reception & SDO (50m<sup>2</sup>)

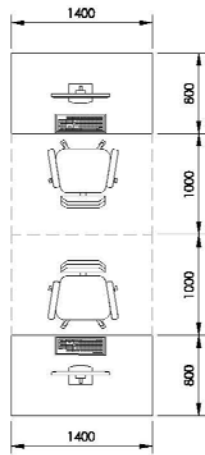
### Thames Valley Police – OPTION 2 (3 storeys)

Name	Number	Area m <sup>2</sup>	Total m <sup>2</sup>		
<b>Workstations</b>					
Resident Workstation	10	2.5	25		
Ops highly Mobile Workstation	21	2.5	52.5		
<b>Open Plan</b>					
Touchdown Workstation	3	1.6	4.8		
Personal storage	51	1	51		
Break Out Space (informal meeting)	2	6	12		
<b>Cellular Office</b>					
Office Type 2	1	9	9		
<b>Ancillary Office</b>					
Study Booth	3	4.5	13.5		
<b>Ancillary</b>					
Locker room (105 lockers)	1	67.5	67.5		
Filing Room (records)	1	20	20		
8 -10 person Meeting Room	1	20	20		
Voluntary Interview Room	1	10	10		
Tea Point (kitchenette)	1	6	6		
Print / Photo Copy Station	1	2.4	2.4		
<b>Support</b>					
Silver Suite (12-18 person meeting room)	1	85	85		
Visitor Touchdown (in reception)	2	1.6	3.2		
ICT Comms. Room	1	9	9		
CCTV Server Room	1	5	5		
Lap top charging point	1	2	2		
Faith Room	1	9	9		
Reception & SDO Type 2	1	50	50		
Property / Evidence Store	1	15	15		
Changing Cubicle (in locker room)	1	3	3		
Shower Room	2	3	6		
Drying Room	1	6	6		
Cleaners Store	1	3	3		
WC	7	5	35	3 x male, 3 x female, 1 x disabled	
Plant Room	1	22	22		
<b>Storage</b>					
Cone Store	1	10	10		
Taser Store	1	6	6		
Bodyworn Video & CCTV Review	1	9	9		
Post Room	1	2	2		
PSU Bag Store	1	15	15		
Trap Bikes	1	9	9		
Leaflet Store	1	3	3		
		NIA	600.9	circulation (say 15%)	90.1
		GIA	691.0		
Secure Car Parking Space	19	(2.8m x 4.8m)			
Van Parking Space	1	(3.0m x 5.8m)			
Public Parking Space	35	(2.4m x 4.8m)			
Cycle Parking	35	(0.8m x 1.85m)			

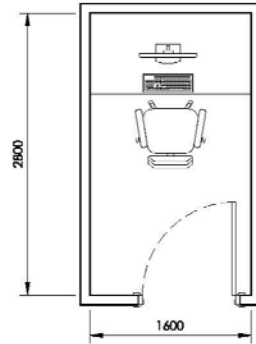
Indicative Room Layouts



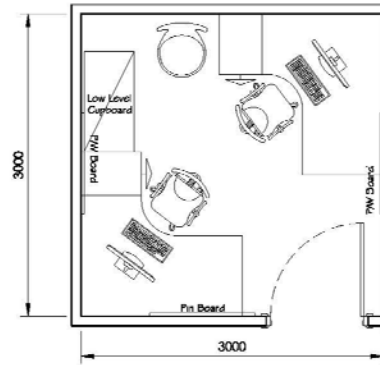
Touchdown (1.6m<sup>2</sup>)



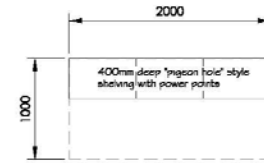
Workstation (resident & mobile)



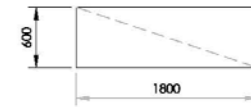
Study Booth (4.5m<sup>2</sup>)



Office Type 2 (9m<sup>2</sup>)

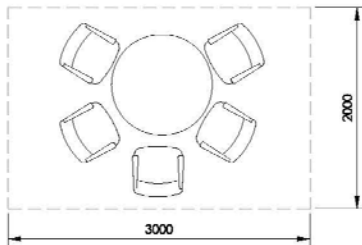


Laptop Charging (2m<sup>2</sup>)

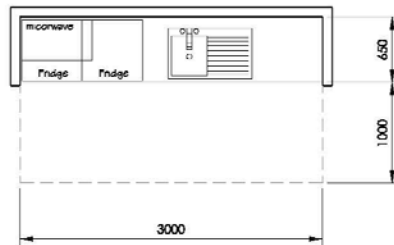


Personal Storage (1m<sup>2</sup>)

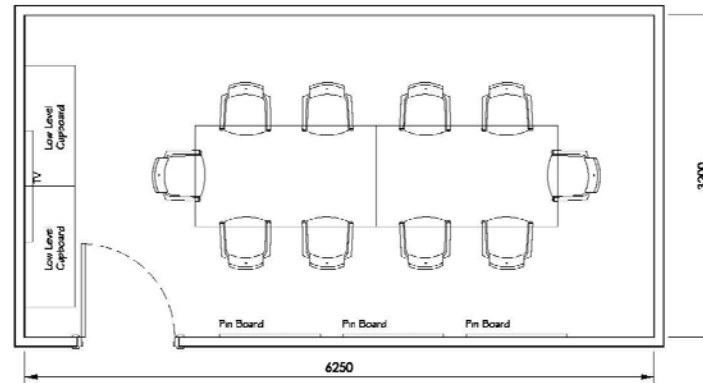
19



Break-out space (6m<sup>2</sup>) informal meeting



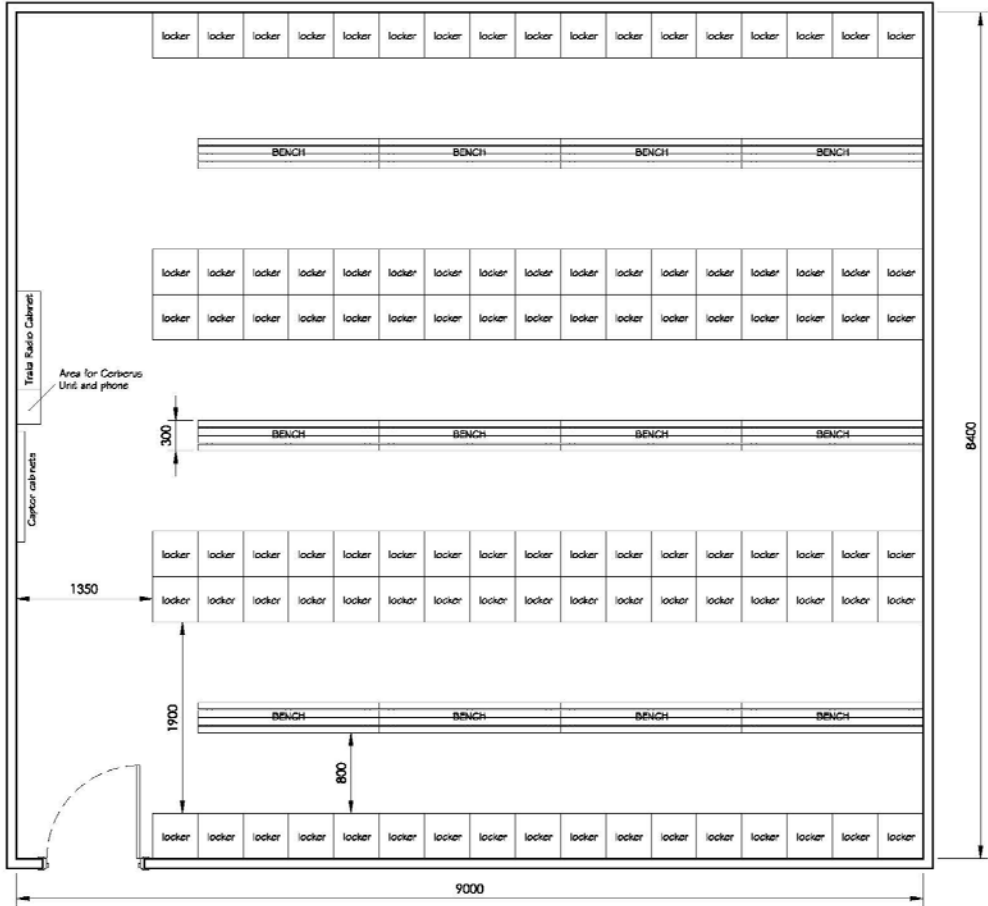
Tea Point (5m<sup>2</sup>)



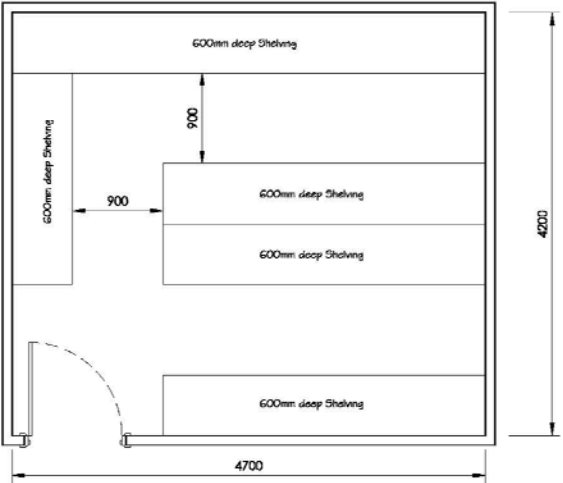
8 - 10 Person Meeting Room (20m<sup>2</sup>)

Indicative Room Layouts

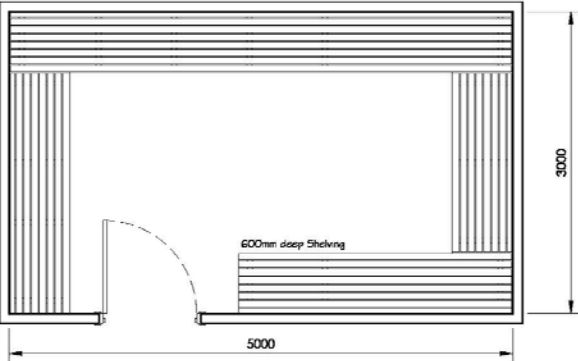
20



Locker Room (75.6m<sup>2</sup>)



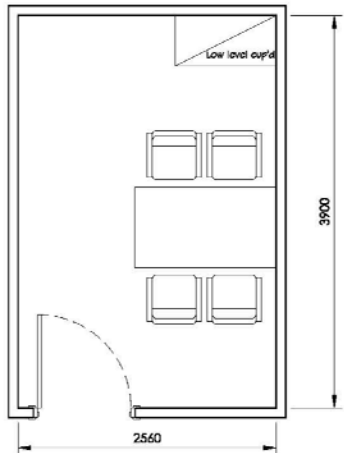
Filing Room - records (20m<sup>2</sup>)



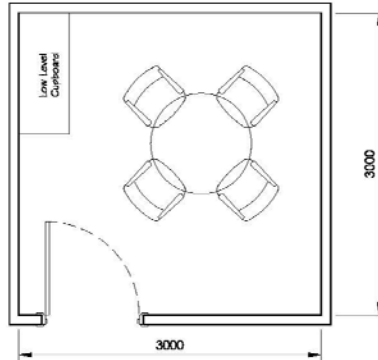
Property / Evidence store (15m<sup>2</sup>)

Indicative Room Layouts

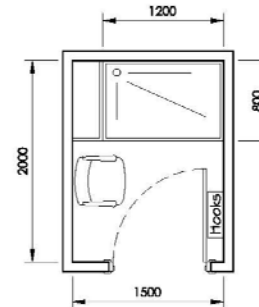
21



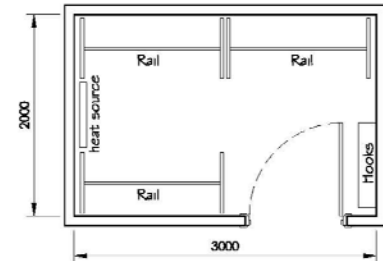
Voluntary Interview Room (10m<sup>2</sup>)



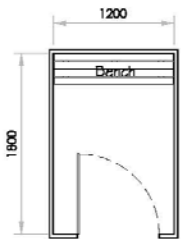
Faith room (9m<sup>2</sup>)



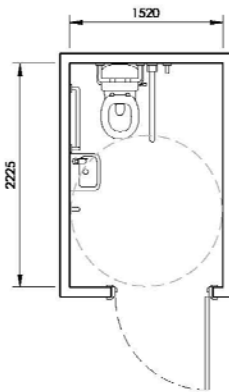
Shower Room (3m<sup>2</sup>)



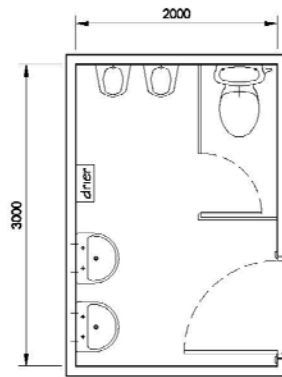
Drying Room (6m<sup>2</sup>)



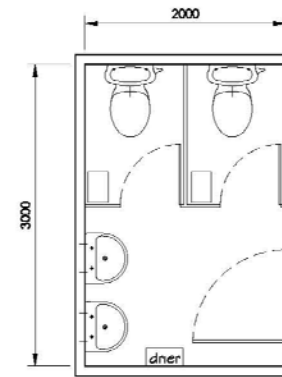
Changing (2m<sup>2</sup>)



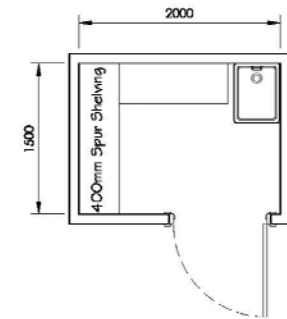
Disabled WC (2.5m<sup>2</sup>)



Male WC (5m<sup>2</sup>)

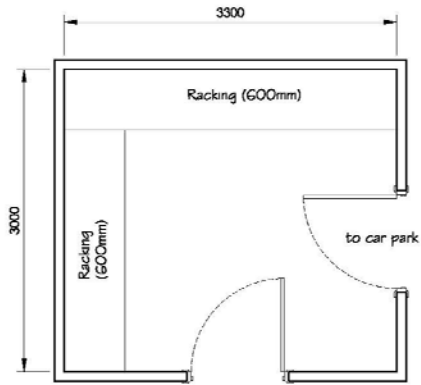


Female WC (5m<sup>2</sup>)

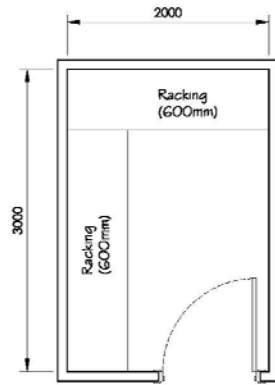


Cleaners Store (3.0m<sup>2</sup>)

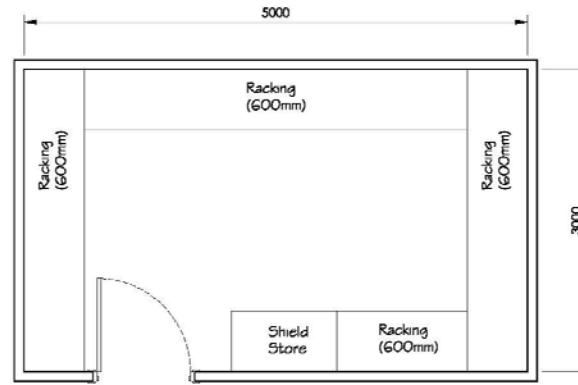
Indicative Room Layouts



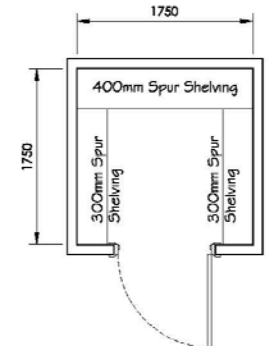
Cone Store (10m<sup>2</sup>)



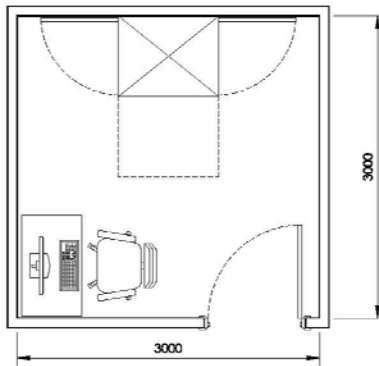
Taser Store (6m<sup>2</sup>)



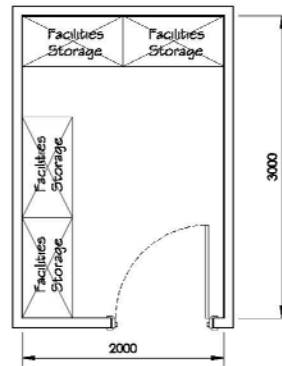
PSU Bag / Equipment Store (15m<sup>2</sup>)



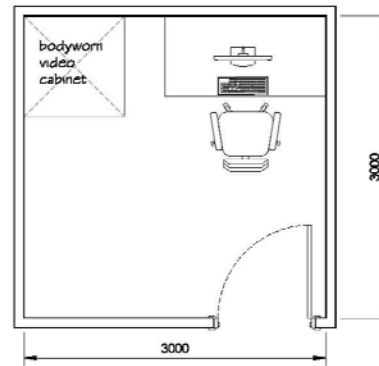
Leaflet Store (3m<sup>2</sup>)



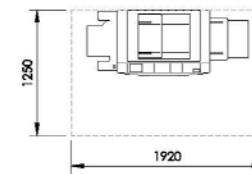
ICT / Comms room (9m<sup>2</sup>)



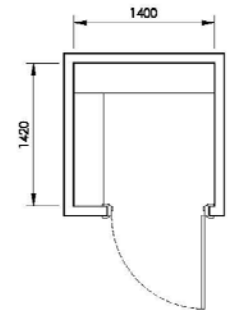
CCTV Server Room (5m<sup>2</sup>)



Bodyworn video / CCTV Review (9m<sup>2</sup>)



Print station (2.4m<sup>2</sup>)

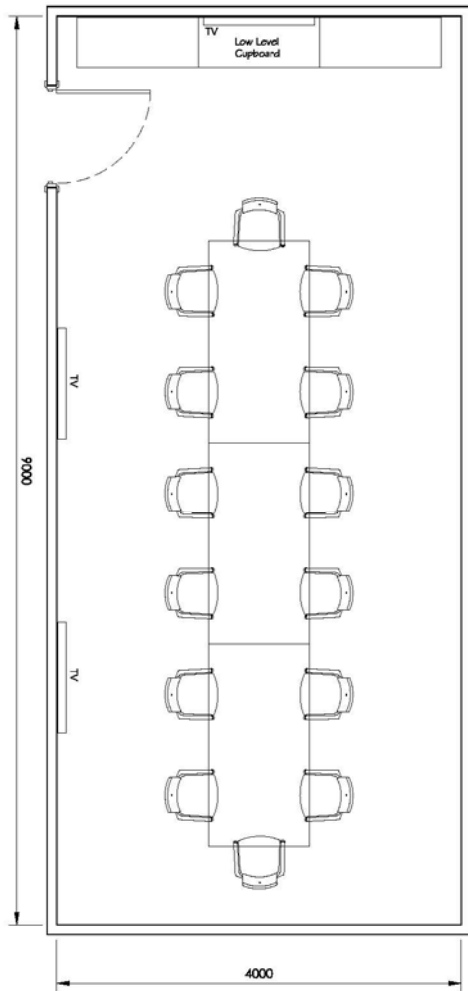


Post (2m<sup>2</sup>)

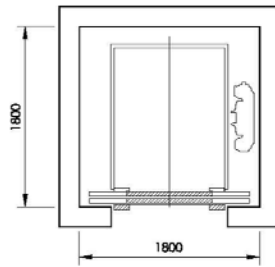
22

Indicative Room Layouts

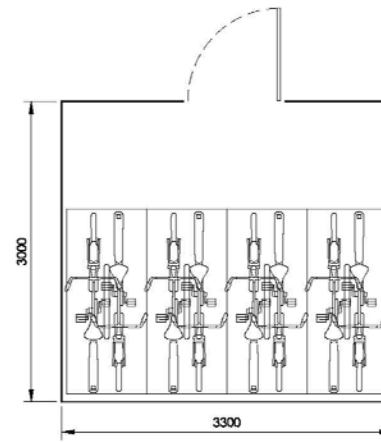
23



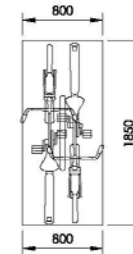
Silver Suite (2 storey Option)  
(36m<sup>2</sup>)



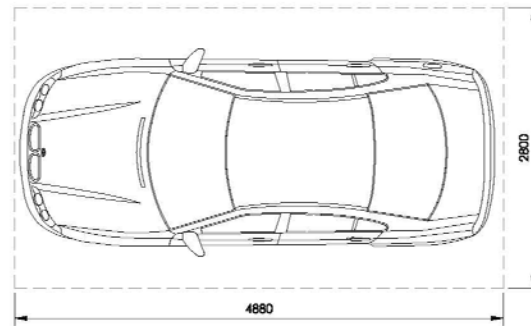
Lift (3.25m<sup>2</sup>)



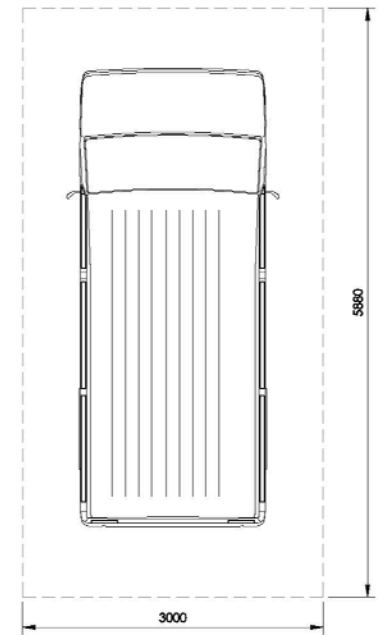
Trap Bike Store (9m<sup>2</sup>)



Cycle Parking x2

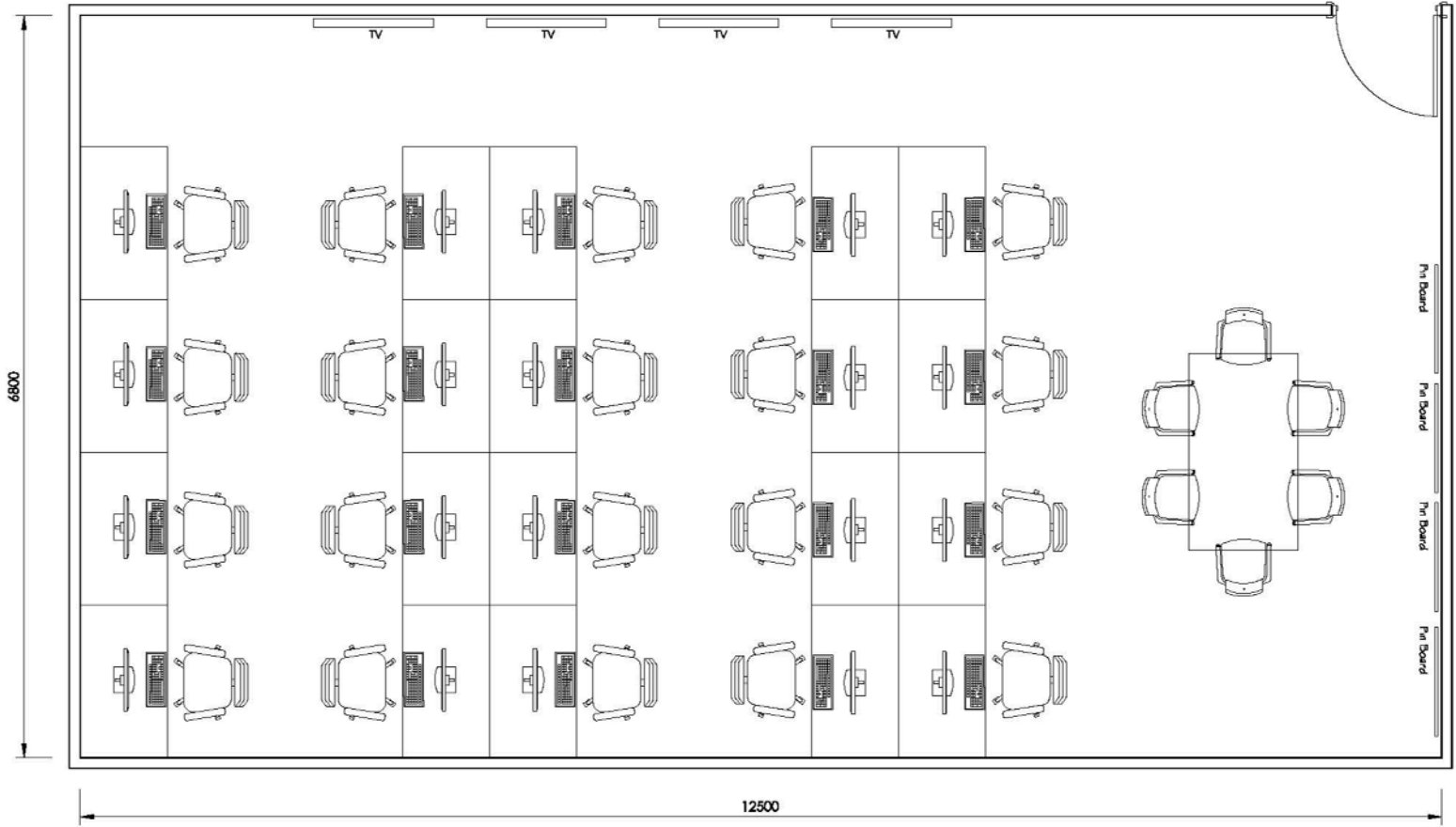


Car Parking Bay



Van Parking Bay

Indicative Room Layouts

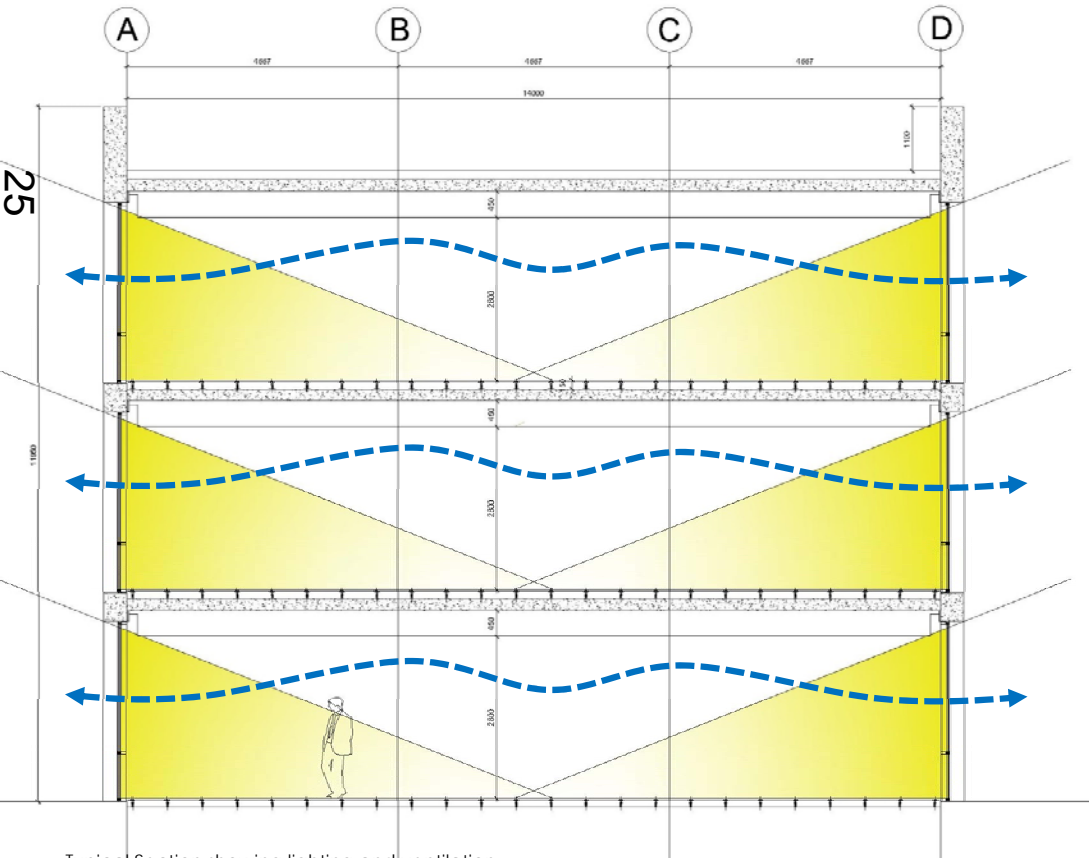
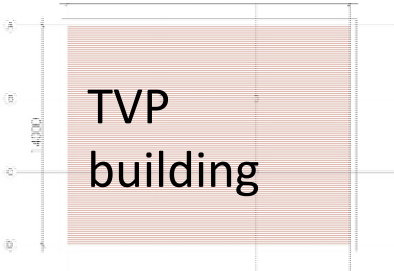
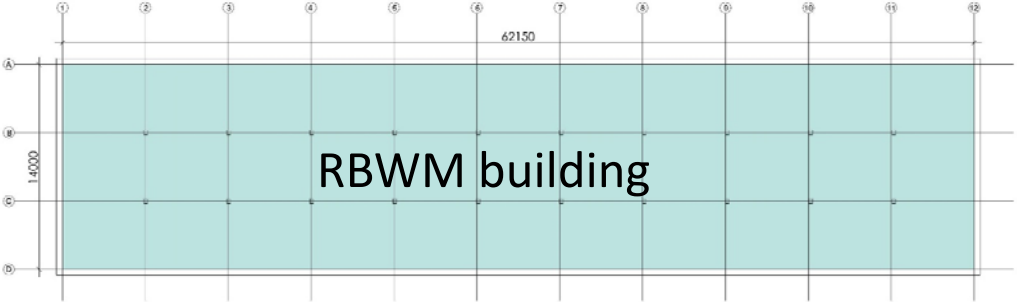


Silver Suite (3 storey Option)  
(85m<sup>2</sup>)

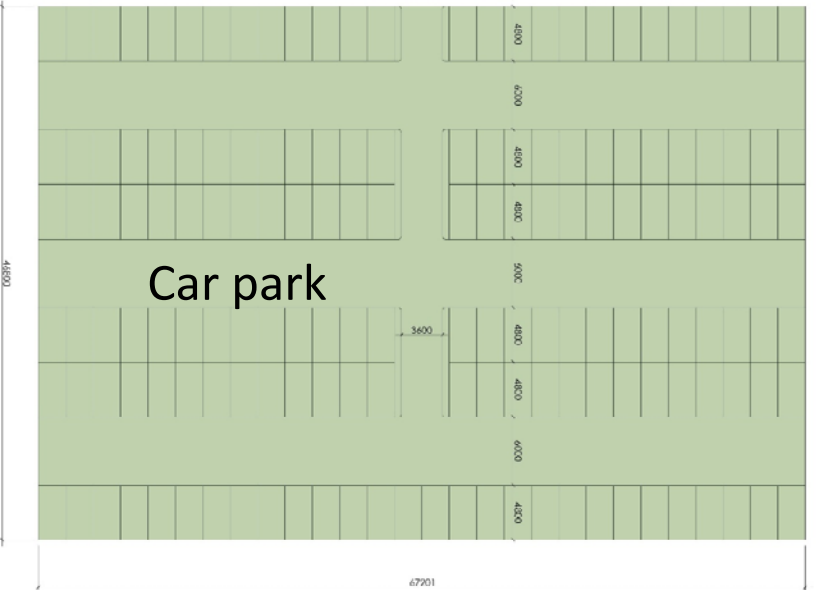
24



Early analysis has determined the footprint of the TVP and RBWM buildings assuming a 3 storey building. The depth of the floor plat has been determined on the principle of maximising natural lighting and ventilation. A ratio of 3:1 (floor plate depth:floor to ceiling height) is the accepted ratio and has been adopted. The following diagrams demonstrate the typical building and car park footprints required to be accommodated on each site.



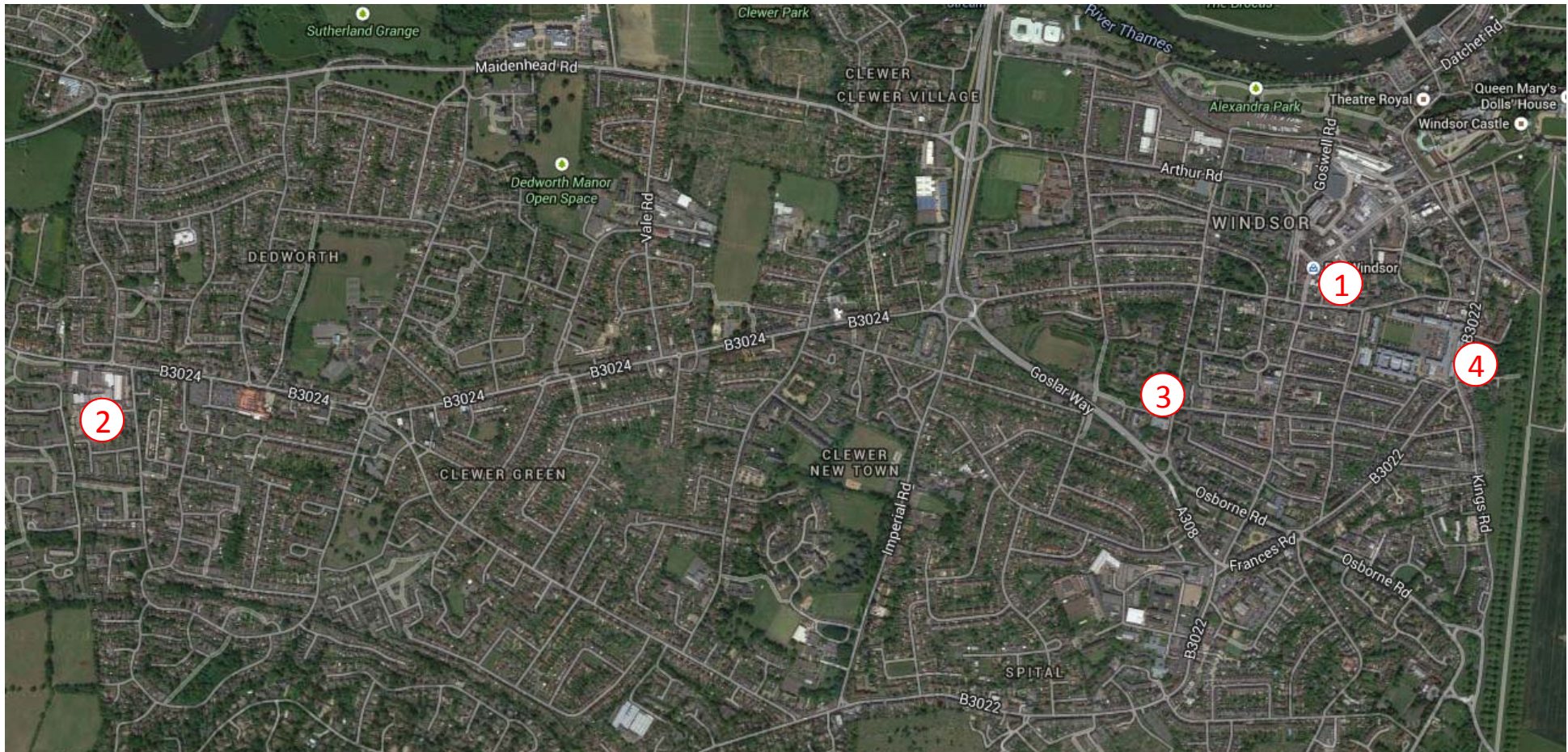
Typical Section showing lighting and ventilation





The sites under consideration within this report are as follows:

1. Windsor Library, Bachelors Acre, Windsor
2. Tinkers Lane Depot, Tinkers Lane, Windsor
3. TVP Police Station Site, Alma Road, Windsor
4. RBWM office, York House, Sheet Street, Windsor





SITE 1

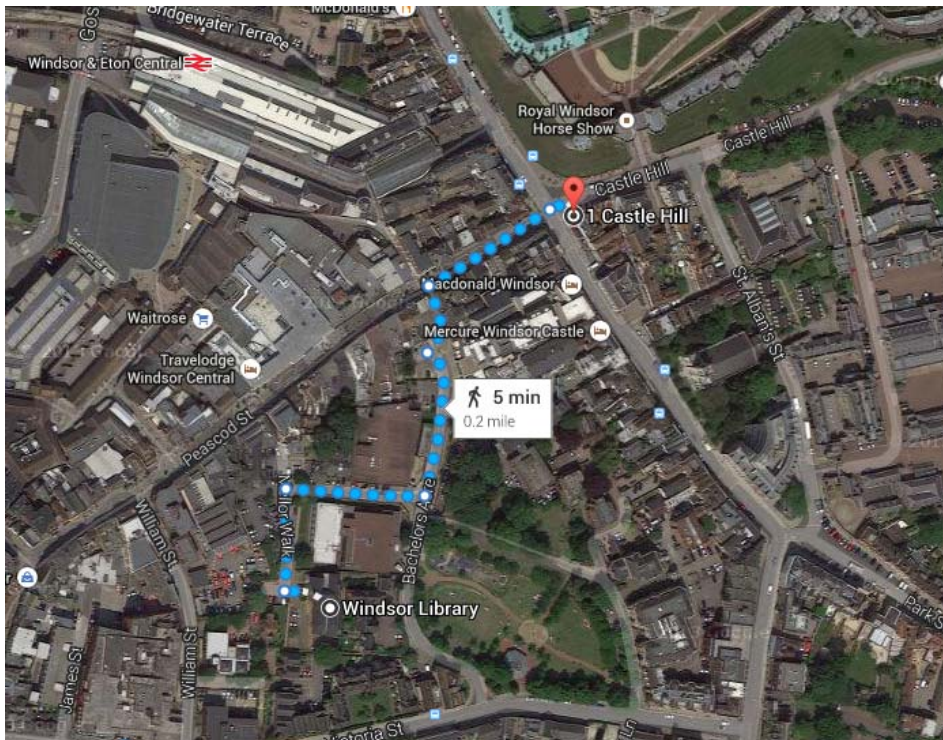
Windsor Library  
Bachelors Acre

**SITE 1 – WINDSOR LIBRARY, BECHELOR'S ACRE**

This site is the current location of Windsor Library and is 0.2 miles from the centre of Windsor (approximately 5 minutes walk).

Vehicular access is via Bachelor's Acre which is a narrow, dead end road leading uphill from Victoria Street towards The MacDonal Hotel Car Park and a pedestrian link onto Peascod Street. Melor Walk leads from Bachelors Acre to the front of the library

The site is adjacent to Bachelors Acre Park which contains a children's play area, ornamental fountains, green open space and a bandstand containing a small shop and public conveniences.



Site Location in relation to "town centre"



Birds Eye View of Site



Front of existing library from Bachelor's Acre



Bachelor's Acre Park Opposite Site



Front of existing library from Melor Street



Junction onto Bachelors Acre from Victoria Street

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Pros	Cons
Central Location right in the centre of town	No main street frontage
Easily accessible by pedestrians	Narrow access for number of parking spaces required on site
	Site Area is very small and unable to accommodate full development
	Site Area is not large enough to accommodate the number of parking spaces required for the development
	Congested site for construction traffic access
	Potential rights of light and party wall issues with neighbouring sites

#### CONCLUSION

This site has been dismissed as a viable option due to its size and location. The site is too small to accommodate a new police station and a new council office or the required parking for each use. It is felt that the congested nature of this site would also increase construction costs.

**No further development options have been considered on this site.**



SITE 2

Tinkers Lane Depot

**SITE 2 – TINKERS LANE DEPOT SITE**

This site is the current location of the Local Authority Depot containing a small office, CCTV Control Centre (for the entire Borough) and storage. It is 2.5 miles from the centre of Windsor (approximately 51 minutes walk).

Vehicular access is via the B3204 (Dedworth Road) onto Tinkers Lane.

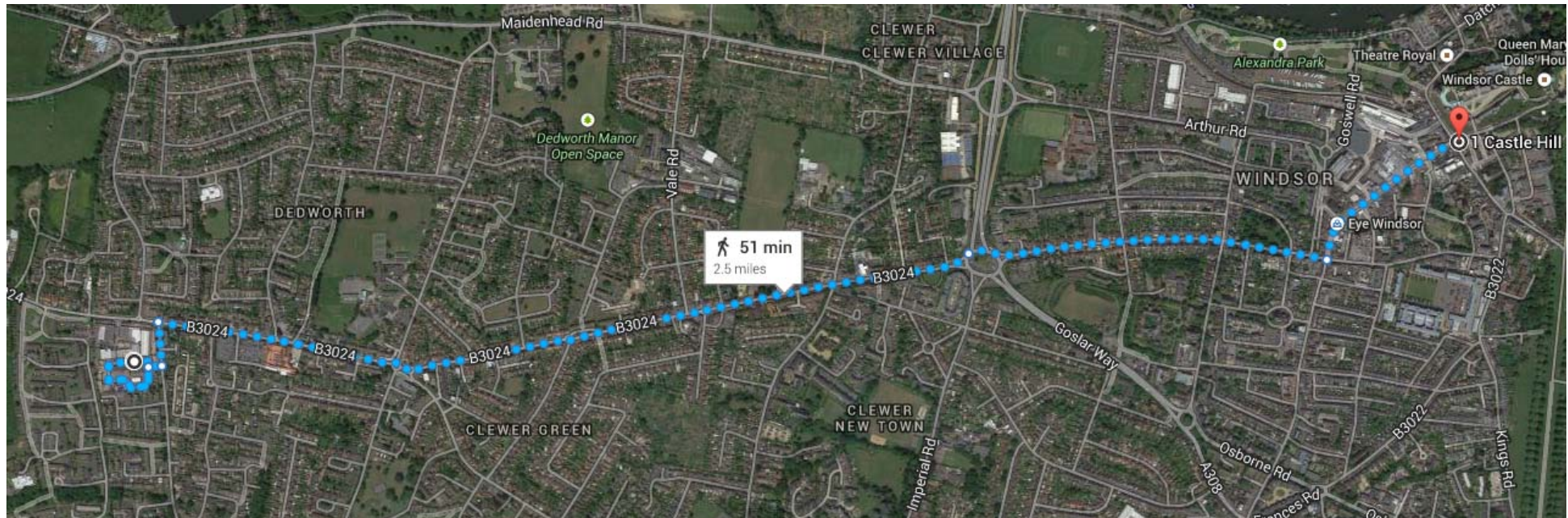
This site directly to the north is the 2 storey Fairacres industrial / warehouse / office accommodation facing directly onto Dedworth Road. The surrounding area is generally 2 storey residential housing with pitched roofs.

Part of this site has been allocated to the new Windsor Fire Station which is relocating from St Mark’s Road in the centre of Windsor.

The remainder of the site contains single storey storage accommodation to the periphery and a small two storey office in the centre with extensive IT equipment serving the wider area.



Birds Eye View of Site



Site Location in relation to “town centre”

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Site Access from Tinkers Lane



Recycling Area designated for new Fire Station

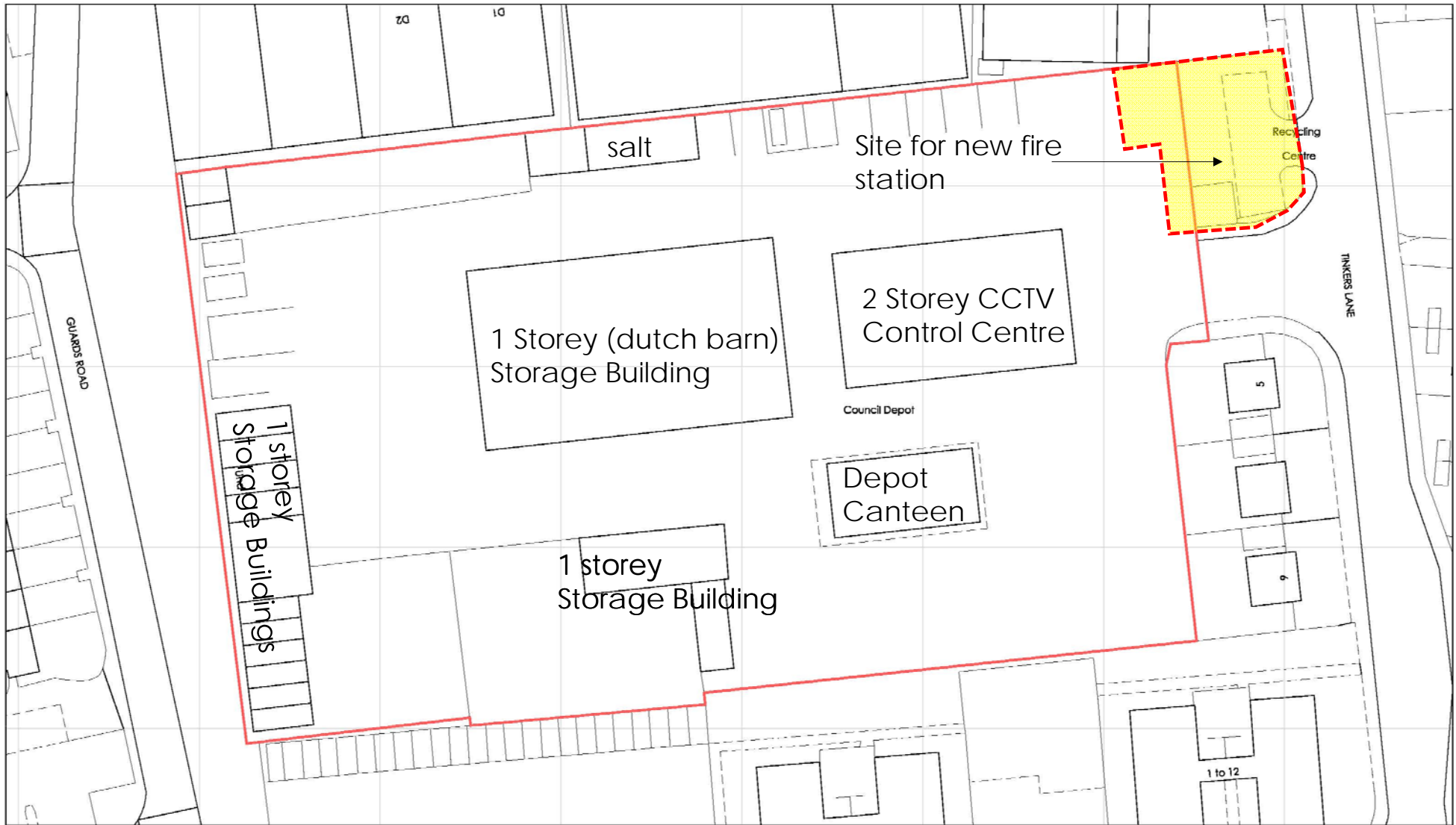


Rear of Site Looking South Along Guards Road



Rear of Site Looking North Along Guards Road

# 5.0 Overview of Development Sites – SITE 2

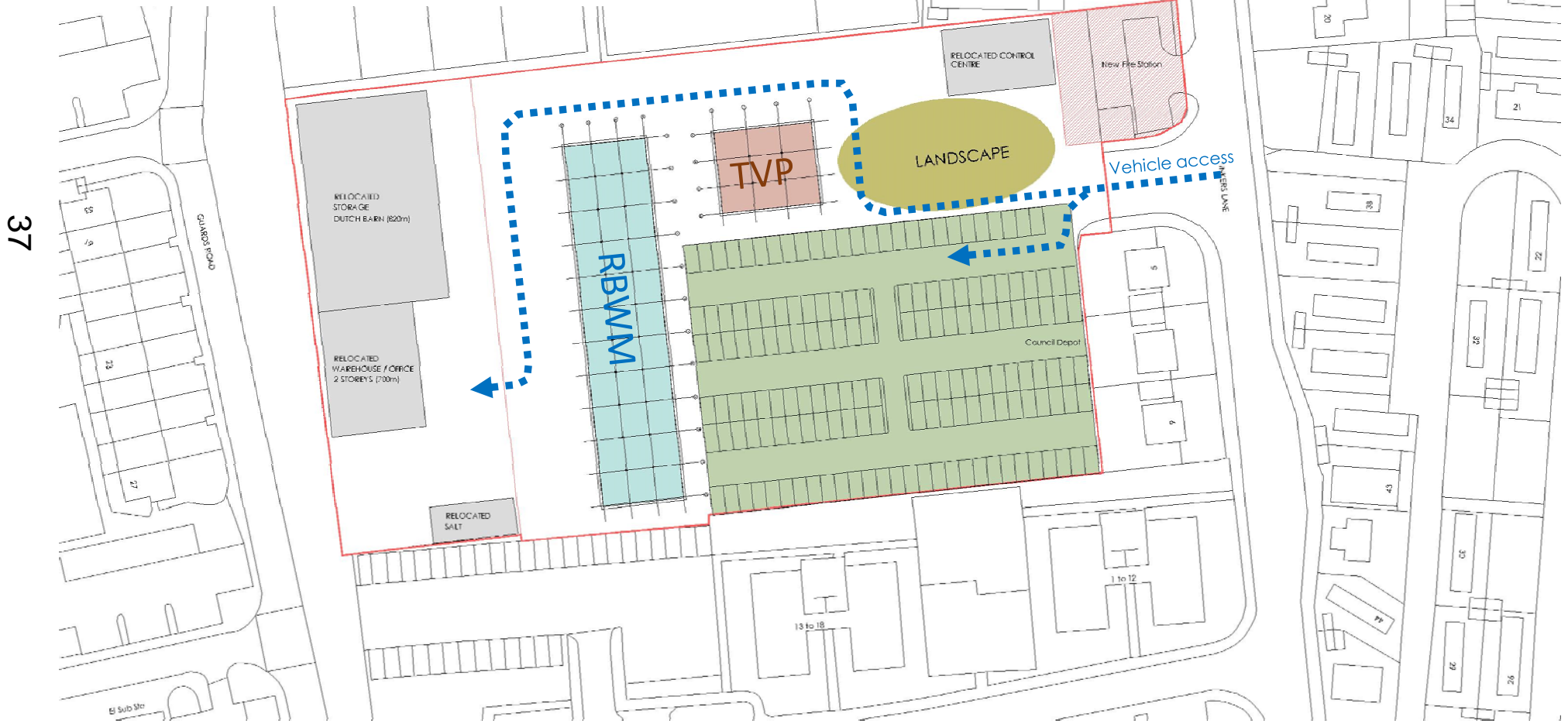


36

Existing Site Plan

This site is flat and easily accessible from the B3204. The IT control centre is active and would need to be re-provided in the event of redevelopment of this site. The following site loading diagrams have been prepared to determine the site's capacity to accommodate the proposed TVP and RBWM buildings and associated parking.

It should be noted that any redevelopment of this site will require the re-provision of the IT facilities as well as some of the storage, warehouse and office space. An assumed re-provision has been included within this feasibility.

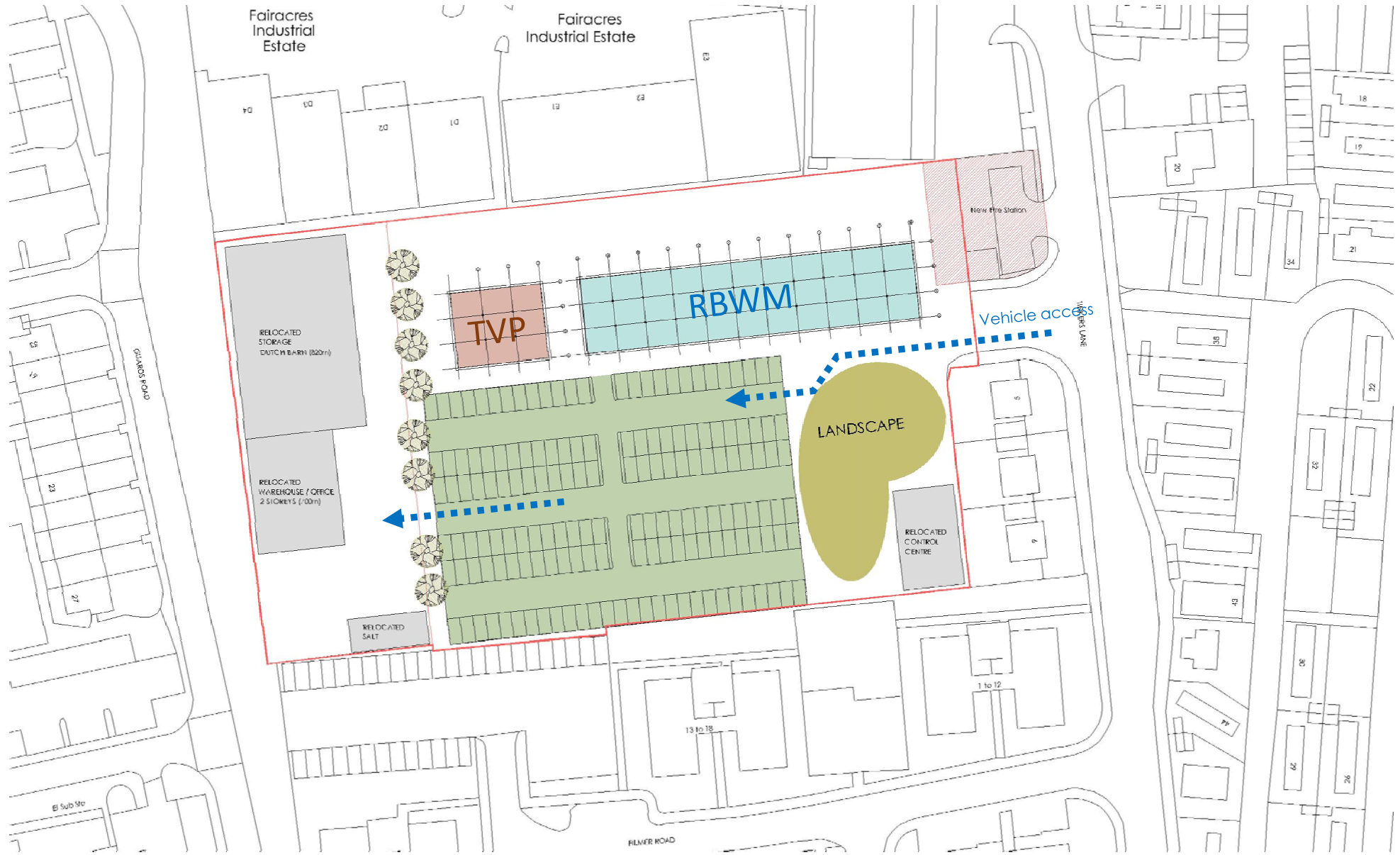


Redevelopment Option 1

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# 5.0 Overview of Development Sites – SITE 2

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Redevelopment Option 2

# 5.0 Overview of Development Sites – SITE 2

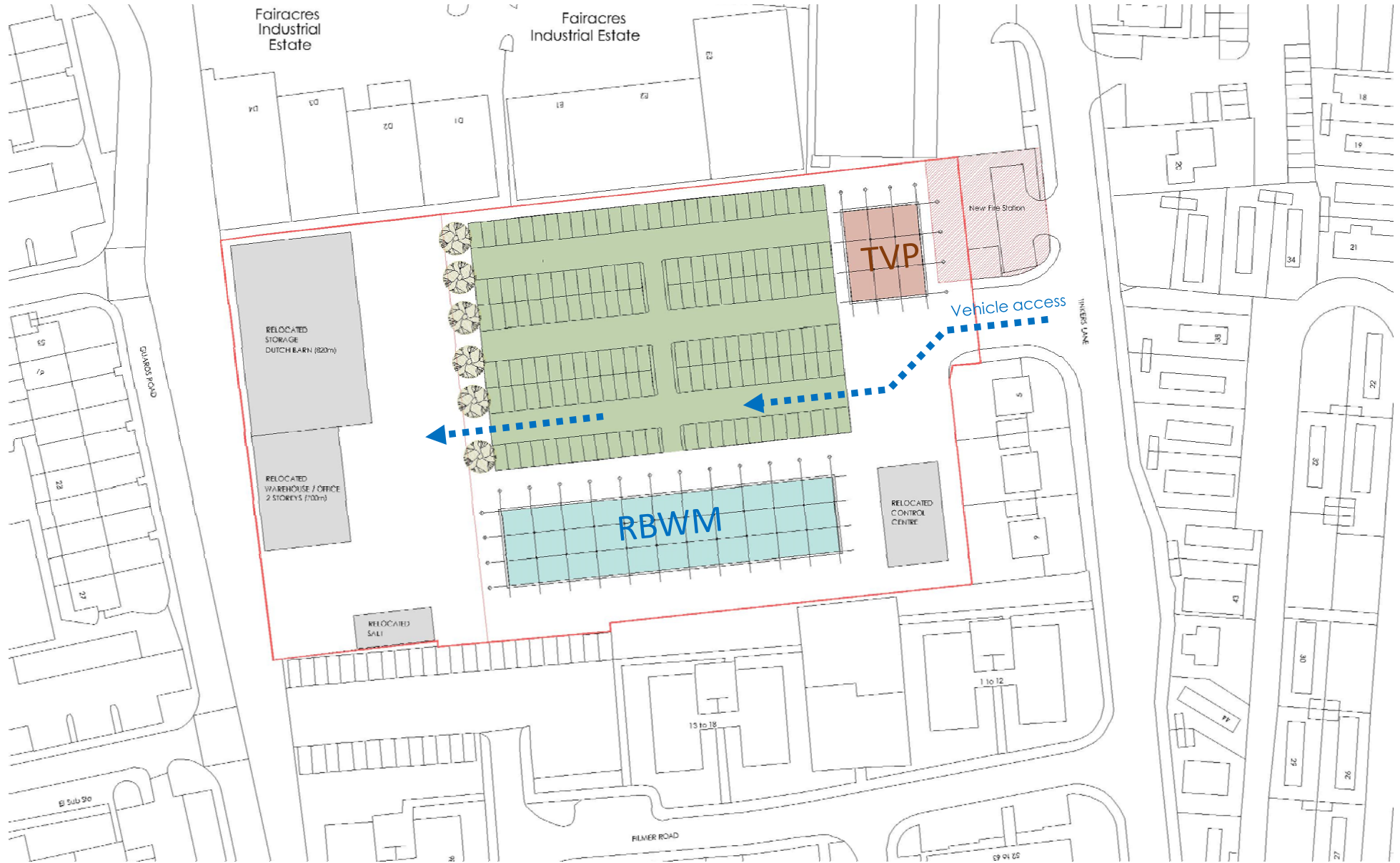
39



Redevelopment Option 3

# 5.0 Overview of Development Sites – SITE 2

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Redevelopment Option 4



## PLANNING APPRAISAL

### Executive summary

- The site is located within the Fairacres Industrial Estate which is subject to adopted planning policy that seeks to protect its continued use for industrial and small scale distribution and storage uses (Use classes B1 (c), B2 and B8). Policy is clear that the Council will not permit development, redevelopment or change of use for any use other than that relating to employment.
- Notwithstanding the above, our review of the Council's Planning Register has identified that planning permission was granted in 2013 for part of the site for use as a satellite fire station and that 'need' was an important consideration in the context of justifying the location of the proposed fire station and hence granting a non-employment use.
- Consultation with stakeholders and the public is likely to be important to the prospects of any scheme forthcoming. For example it will be important to fully articulate the operational requirements of the police station which have determined this site as an appropriate location. This was an important consideration as part of the recently permitted fire station.
- **Based on the constraints and policy considerations identified within this appraisal we would recommend option 3 is progressed if this site was selected as the preferred option given that the police station and office elements are sited in positions that have least impact on neighbouring properties when compared to the other options**
- **The planning risk associated with the redevelopment of the site for replacement Council offices and a new police station, as envisaged through 3, is considered to be medium**, primarily due to land use policy constraints. The planning risk could be reduced to low subject to locational arguments advanced in respect of the Police Station needing to be located at this site.

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### Site description

The site is located to the west of Tinkers Lane and is known as the Tinkers Lane Council Depot. It is used as a recycling centre and car parking and forms part of the wider Fairacres Industrial Estate which extends north up to Dedworth Road.

With the exception of 2 storey residential properties the site's eastern boundary abuts Tinkers Lane with the rear of the Castle Farm Caravan Park beyond. The recently permitted fire station is proposed along this boundary (to the north). To the south of the site is a wider residential area estate which is characterised by 2 storey residential properties. The site's western boundary is parallel to Guards Road with 2 storey residential properties formed around a series of cul de sacs beyond.

### Planning history

Our review of the Council's online planning register has identified a number of small scale applications relating to the buildings lawful use for storage and distribution (Use Classes B2 and B8). Of particular relevance to the redevelopment proposal is the approval of the fire station to the front of the Depot. This application has established that the 'need' for a fire station was sufficient to justify redevelopment of part of a site otherwise safeguarded for employment use and that the siting of the building was important in removing concerns regarding impact on existing residents amenity. For the sakes of completeness we have summarised below the applications identified.

- 14/00502/FULL Provision of new modular fire station and relocation of existing recycling depot. Amended design to planning permission 13/01536 Permitted 31/03/2014.
- 11/01465/TLDT Application for a determination as to whether prior approval is required for installation of a replacement 15 metre telecom pole with 6 antennas and 2 transmission dishes Permitted 11/06/2011
- 99/77876/TLDT Application for a determination as to whether prior approval is required for installation of three cross-polar antennas on existing 15m pole together with new equipment cabin. Permitted 09/04/1999.
- 93/01830/REG3 Erection of one single storey portacabin and one two storey portacabin to provide additional office accommodation. Permitted 05/11/1993.

### Planning Policy Context

The redevelopment of the site is subject to those policies contained within the adopted Development Plan for The Royal Borough of Windsor and Maidenhead which comprises the Local Plan (Incorporating Alterations) – Adopted in June 2003 (saved policies).

The Local Plan is supplemented by further local level guidance in the form of SPD's and SPG's which we refer to as necessary throughout the following commentary. Additionally, 'material considerations' are also to be accounted for as part of the determination process. This includes the National Planning Policy Framework (2012) and associated National Planning Practice Guidance (2014).

### Emerging Local Plan

In addition to the above emerging planning policy in the form of an updated Borough-wide Plan, Site Allocations DPD and Neighbourhood plan is progressing through the adoption process. Whilst these documents set out the policy direction of travel they are at early stage of the adoption process and so have been considered accordingly.

### Site & Area Specific Planning Policy

The following area and site specific policy designations apply to the site and its future redevelopment.

**Employment Area (Fairacres Industrial Estate)** – The site forms part of the wider Fairacres Industrial Estate which is safeguarded for Industrial and Small Scale Distribution and Storage uses. There is a strong policy presumption against development, redevelopment or change of use of such sites other than for business, industrial or warehousing.

There are no other site or area wide policy designations relevant to the redevelopment of the site.

Relevant 'saved' policies include:

- Policy DG1 (Design Guidelines)
- Policy E1 (Employment)
- Policy H3 (Affordable housing within urban areas)
- Policy H10 (Housing layout and design)
- Policy NAP3 (Polluting Development)
- Policy T5 (New Developments and Highway Design)
- Policy P4 (Parking within Development)
- Adopted Parking Standards

### Emerging Options Review and Relevant Policy Considerations

Having identified the relevant site and area-wide policies we have summarised below those 'thematic policies' which have been considered alongside the 4 no. development options prepared in this report on previous pages.

**Land Use** – the proposed replacement Council offices are acceptable in principle when considered against relevant planning policy and subject to identifying an appropriate alternative location for the current recycling depot. The proposed police station is however likely to be opposed as this does not constitute an 'employment generating' use as defined by the Local Plan. Whilst the permitted fire station sets a useful precedent in that it was not considered 'employment generating', it was the case presented in terms of 'need' and in particular catchment that allowed for a departure in policy in terms of land use. If such an argument could be advanced in respect of the police station then clearly it may be possible to provide such a use on site but it is at this stage understood that a more central location is important.

**Quantum** – The height of development within the site and the locale is relatively modest, ranging between 1 and 3 storeys in height. The emerging proposal (s) which envisage a height of 3 storeys are therefore likely to be acceptable in principle. Notwithstanding, the scale of development and absence of any policy relating to height and in particular heritage assets suggests that additional height could be achieved, possibly to the centre of the site.

In terms layout options, it is considered that option 3 represents the preferred approach in respect of planning policy, primarily on the basis that the proposed police station is located to the north east of the site. This is the furthest point from the adjacent residential properties and adjacent to the recently permitted fire station. As part of the approval of the latter potential issues regarding impact on amenity were removed due to the location of the fire station to the north east of the site. This option also includes an area of landscape to the rear of properties fronting Tinker Lane which will help improve the appearance of the site and transition of uses. Notwithstanding, it is suggested that the proposed office block is 'pulled' further away from the residential properties to the south. Provision should also be made for vehicular access to the surplus land to the rear of the site so that it can be utilized in the future.

Whilst option 4 also locates the proposed police station to the north east of the site the configuration of the office block is such that the longest part of the building backs onto the residential properties to the south which may give rise to visual impact and overlooking.

**Design** – There are no prevalent architectural styles within the local nor heritage assets that would dictate a certain design approach.

**Parking & Highways** - Relevant planning policy requires 1 vehicular space per 100 sq.m of commercial floorspace. There is no standard for police stations but we would assume the commercial figure as a starting point. In addition, 2% of all car parking spaces should be provided for visitors and there should be 1 motorcycle space for every 20 vehicular spaces. Cycling parking should be provided at 1 space per 20 employees. It is suggested that the current parking analysis scheme (above) be revisited in light of these requirements. The above standards assume the Council agrees that the site is classified as being within a highly sustainable location  
 The Local Plan is supplemented by further local level guidance in the form of SPD's and SPG's which we refer to as necessary throughout the following commentary. Additionally, 'material considerations' are also to be accounted for as part of the determination process. This includes the National Planning Policy Framework (2012) and associated National Planning Practice Guidance (2014).

**Planning Conclusion**

The principle of the proposed office use is acceptable but the police station may be opposed given that it is not an employment generating use and currently it is understood that there is no overriding need for it to be located at this site in terms of catchment. If it could be argued that there is a need to locate a police station on this particular site then a departure from policy could be allowed.

**SUMMARY**

The overall site area of the Tinkers Lane site is 11,188m<sup>2</sup>. Approximately 500m<sup>2</sup> is required for the redevelopment of the Fire Station which leaves 10,688m<sup>2</sup> for the redevelopment of the TVP and RBWM buildings.

The preliminary studies suggest that the new police station, council offices, re-provided control centre and parking can be accommodated within 8,188m<sup>2</sup>. This leaves approximately 2,500m<sup>2</sup> for the re-provision of storage facilities or additional development.

Redevelopment of this site will require the IT facility to remain operational throughout to ensure continuity of service. This will have an adverse affect on site availability and work sequencing as the new facility will need to be constructed and fully operational before the existing facility, in the middle of the site, can be demolished. Existing storage facilities and some office space will also need to be re-provided.

Pros	Cons
Good sized site which can accommodate all required buildings and parking	No main street frontage
Limited planning risk for proposed development	The site is too far out of town to be considered as a central police station (not considered "Windsor")
The site is flat	The site is too far out of town to walk to.
The site is easily accessible by car	The IT control centre will need to be re-provided BEFORE any redevelopment can take place.
	Additional premises will be required by TVP and RBWM in the town centre to provide public drop in facilities.

**CONCLUSION**

This site has been dismissed as a viable option due to its location. 2.5miles outside of the centre of Windsor cannot be considered as "local". Both RBWM and TVP would need to provide additional public counter facilities in the centre of Windsor to compensate for this location.

The costs of re-providing the IT control centre are also prohibitive.

**No further development options have been considered on this site.**



SITE 3

TVP Police Station  
Alma Road

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**SITE 3 – ALMA ROAD POLICE STATION SITE**

This site is the current location of the Windsor Police Station. The site is currently underutilised and the Police Station is larger than required for the current operations from this base. It is considered that this site is too large for sole use by TVP and the opportunity for redevelopment exists to maximise the potential of the site.

The existing 5/6 storey building looks run down and is not in a particularly good state of repair. Its appearance is also out of scale and character with the surrounding area. Some single storey storage buildings are located on site to the north of the car park to the rear. The site is flat.

There are some parking bays directly in front of the station but vehicular access to parking at the rear is from the side road leading to Alma Road Social Club. The car park can currently accommodate 63 vehicles.

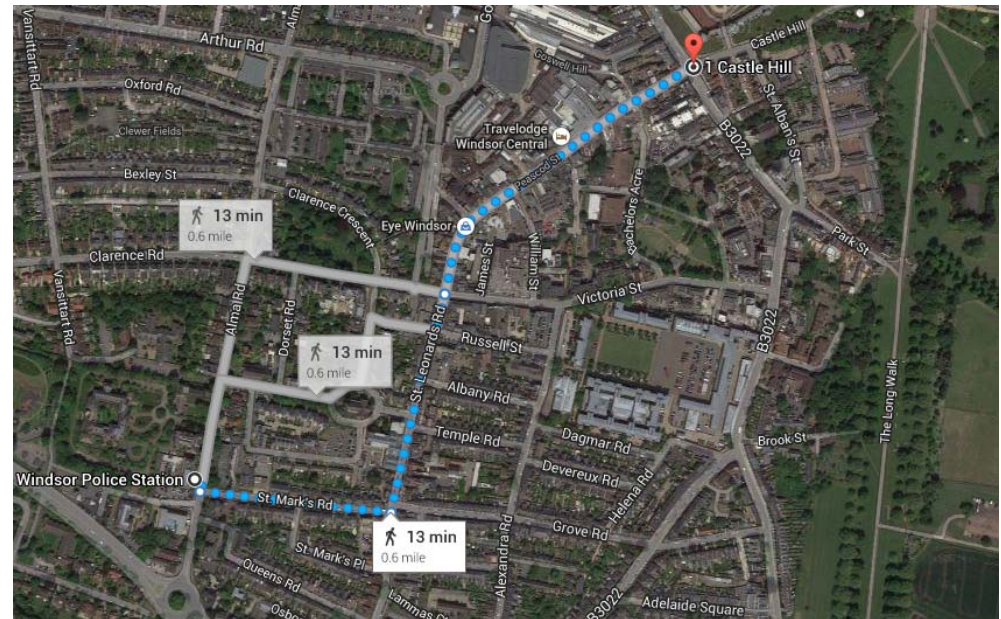
The majority of the surrounding area is residential. Large detached and semi detached houses stretch along Alma Road to the north with a new Holiday Inn Hotel directly to the south. It is worth noting that there have been reports of many of the existing residential basements in Alma Road flooding in December 2013/January 2014

Alma Road is fairly central to Windsor Town Centre although it currently has no retail facilities and such it cannot be considered a “destination” street within the town. The site itself is a 13 minute walk from the Town Centre.

Planning consent was recently granted on appeal for the redevelopment of the Imperial House site to the west of the police station to provide five buildings of between 1 and 5 storeys and a 3-storey car park to provide 25,464 sqm of office floor-space, a cafe/restaurant, ancillary security and substation as well as associated car parking, delivery drop off, service bay, cycle and motorcycle parking.



Birds Eye View of Site



Site Location in relation to “town centre”



Existing Police Station viewed from the north



Existing Police Station and frontage parking



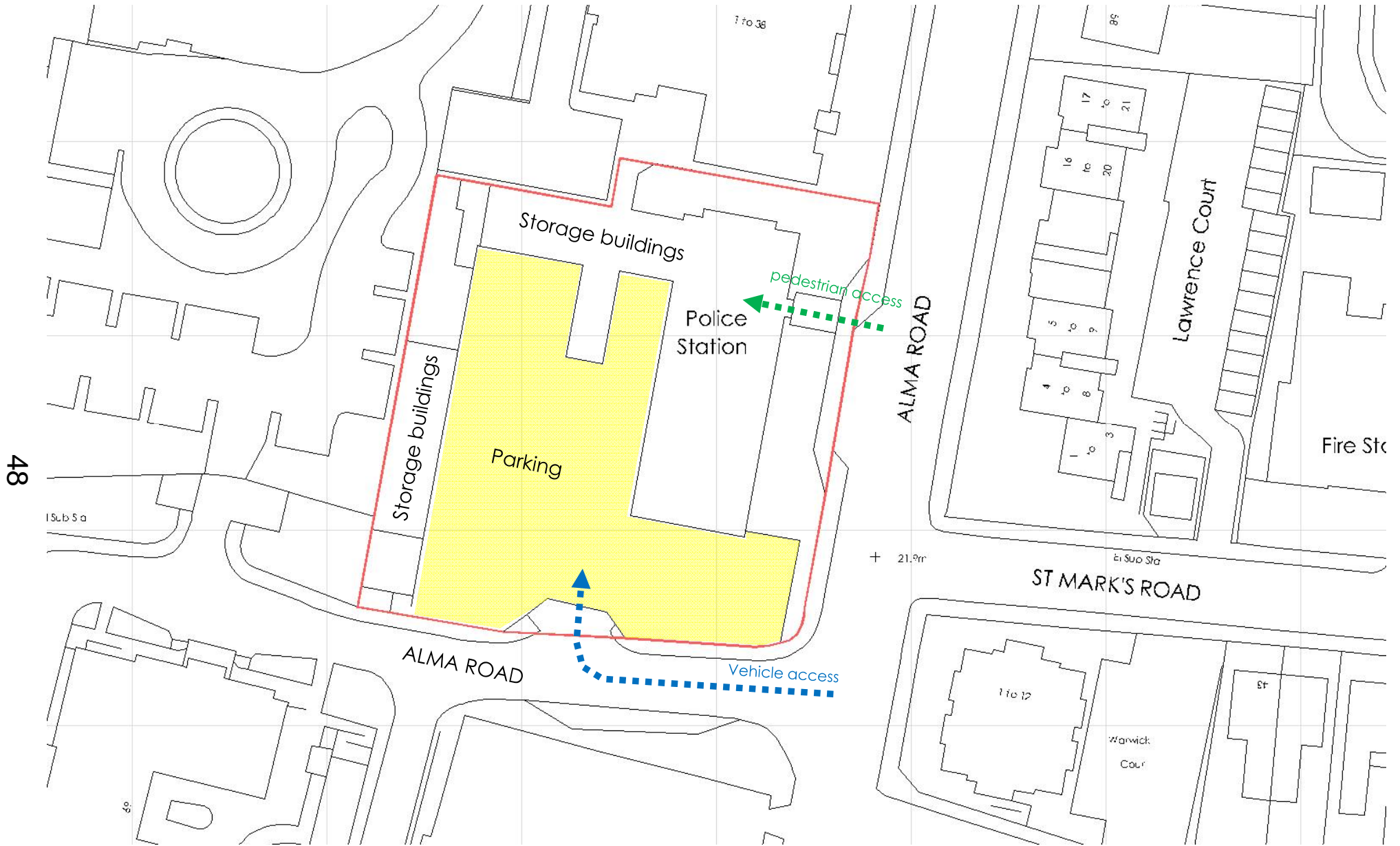
Car parking and storage to the rear of the station



Boundary wall on the corner of Alma Road with refuse storage behind

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# 5.0 Overview of Development Sites – SITE 2



Existing Site Plan



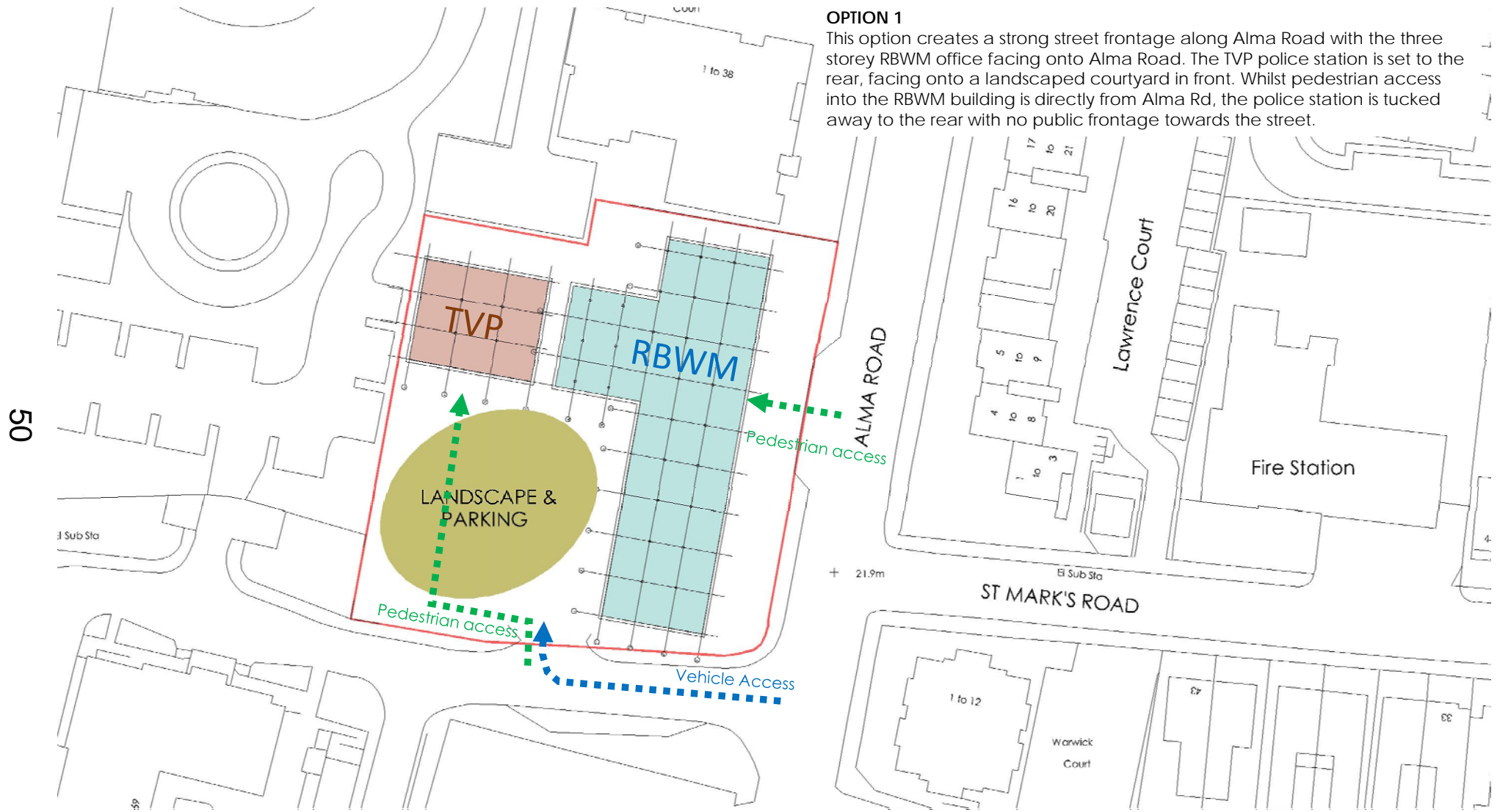
This site is flat and easily accessible from the Alma Road side street. There are various communication masts etc located on the roof of the existing building – any regeneration of this site will need to take into account the termination of these services. The following site loading diagrams have been prepared to determine the site’s capacity to accommodate the proposed TVP and RBWM buildings and associated parking.

**PARKING**

As demonstrated in the basic diagram below, the site at ground level alone is not large enough to accommodate the full parking requirements of the brief. The introduction of building footprints at ground level will reduce car park area further. This site will require a full basement to accommodate the required parking spaces.



Parking Assessment

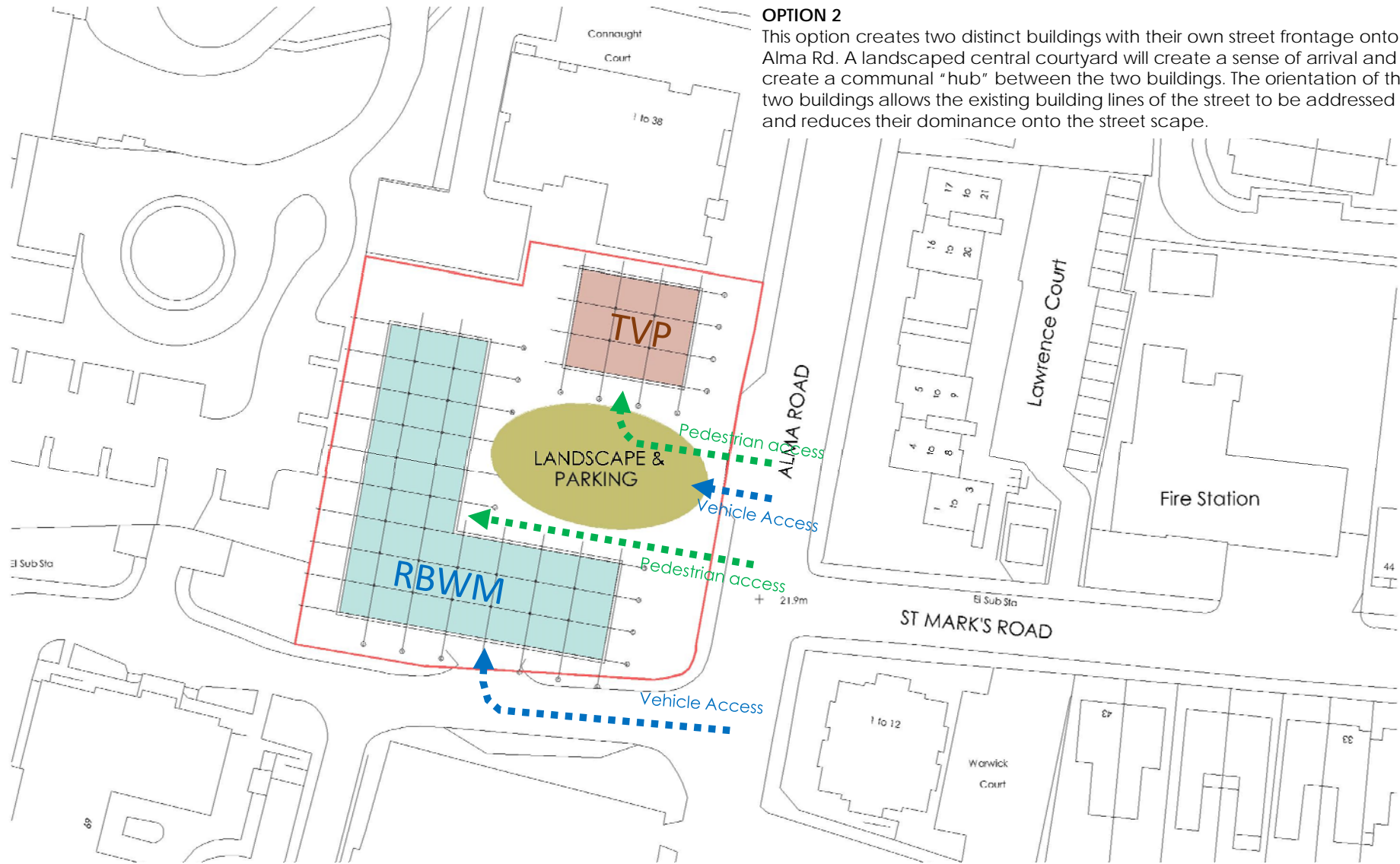


**OPTION 1**

This option creates a strong street frontage along Alma Road with the three storey RBWM office facing onto Alma Road. The TVP police station is set to the rear, facing onto a landscaped courtyard in front. Whilst pedestrian access into the RBWM building is directly from Alma Rd, the police station is tucked away to the rear with no public frontage towards the street.

Redevelopment Option 1

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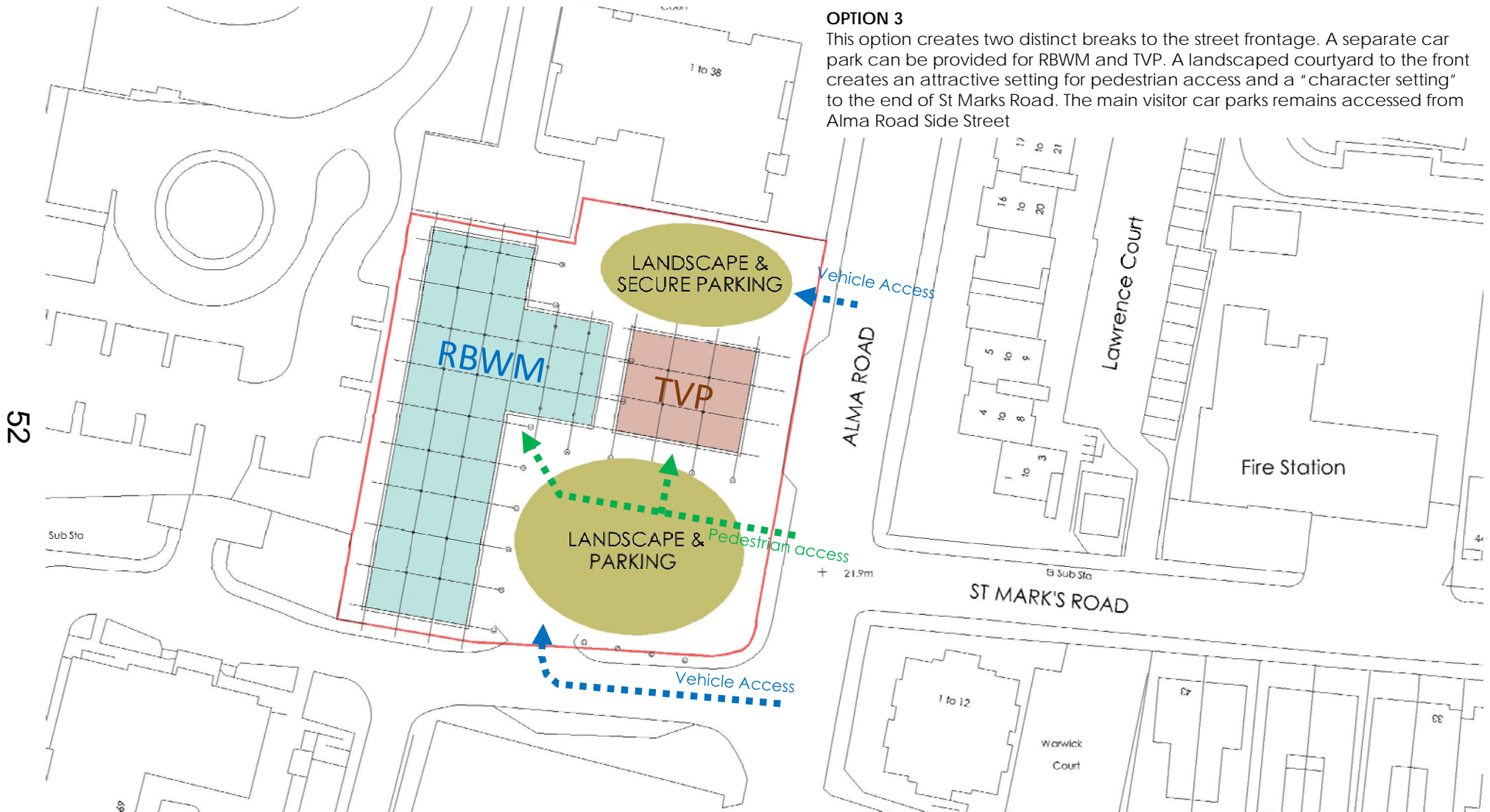
**OPTION 2**  
 This option creates two distinct buildings with their own street frontage onto Alma Rd. A landscaped central courtyard will create a sense of arrival and create a communal “hub” between the two buildings. The orientation of the two buildings allows the existing building lines of the street to be addressed and reduces their dominance onto the street scape.

Redevelopment Option 2

# 5.0 Overview of Development Sites – SITE 3

## OPTION 3

This option creates two distinct breaks to the street frontage. A separate car park can be provided for RBWM and TVP. A landscaped courtyard to the front creates an attractive setting for pedestrian access and a “character setting” to the end of St Marks Road. The main visitor car parks remains accessed from Alma Road Side Street



Redevelopment Option 3

## PLANNING APPRAISAL

### Executive summary

- The site is not subject to any area specific planning policies governing its future or current use and hence its redevelopment for a range of uses is acceptable, in principle.
- Our review of the planning history of the site and relevant proposals suggests that a replacement Police station and Council offices are likely to be acceptable use (s) for the site.
- The site has previously been considered within the Council's Strategic Housing Land Availability Assessment (SHLAA). The SHLAA assessment concludes that medium/high rise flats development is appropriate for the site and based on the Council's initial assessment would yield in the region of 48 units.
- Neither the site or surrounding area are subject to any heritage assets (Conservation Areas or Listed Buildings) which would affect the design or quantum achieved as part of any replacement scheme.
- The initial feasibility studies represent a logical starting position for any replacement proposal. **Each of them has their respective merits but based on the character of the built environment we would recommend option 2 is progressed if this site was selected as the preferred option. In particular, option 2 would create a frontage for the respective use (s) and the creation of the courtyard could be argued to provide visual relief in the built form.**
- **The planning risk associated with the redevelopment of the site for replacement Council offices and a new police station, as envisaged through option 2, is considered to be low**, subject to discounting alternative office locations within the Town Centre, (if required through emerging planning policy).
- If a decision is taken not to proceed with the above proposal a residential development would be appropriate, either through a planning application or promotion through the emerging Development Plan.

### Site description

The site measures approximately 0.13 ha and is located to the west of Alma Road which is approximately 400 metres south of Windsor Town Centre. Residential use characterises the immediate area with a 5 storey residential complex set within landscaped grounds enveloping the site to the north and east. 3 storey purpose built residential apartments are situated to the east of the site beyond Alma Road. A Holiday Inn hotel is situated to the south of the site.

The site itself comprises hard standing used as car parking and service area, a number of garages and outbuildings and a purposes built 5 storey station which provides accommodation to the Thames Valley Policies (3,070 sq.m GIA).

### Planning history

Our review of the Council's online planning register has identified a number of small scale applications relating to the buildings lawful use as a police station (sui generis). There are no applications considered to be of relevance to the redevelopment of the site, albeit there is an application to the west of the site which has established that office use in the locale is acceptable – see below.

- Imperial House site (adjoins the western boundary of the site) planning application (Ref.10/00820/FULL) was recommended for refusal by Council officers. However the applicant appealed (Ref. APP/T0355/A/10/2134960) and the Inspector resolved to allow planning permission for the Demolition of existing building and erection of five buildings of between 1 and 5 storeys and a 3-storey car park to provide 25,464 sqm of office floorspace, a cafe/restaurant, ancillary security and substation as well as associated car parking, delivery drop off, service bay, cycle and motorcycle parking, public accessible open space, improved access and landscaping in February 2011.

### Planning Policy Context

The redevelopment of the site is subject to those policies contained within The Royal Borough of Windsor and Maidenhead Local Plan (Incorporating Alterations) which was adopted in June 2003 (saved policies).

The Local Plan is supplemented by further local level guidance in the form of SPD's and SPG's which we refer to as necessary throughout the following commentary. Additionally, 'material considerations' are also to be accounted for as part of the determination process. This includes the National Planning Policy Framework (2012) and associated National Planning Practice Guidance (2014).

## PLANNING APPRAISAL

### Emerging Local Plan

In addition to the above emerging planning policy in the form of an updated Borough-wide Plan, Site Allocations DPD and Neighbourhood Plan is progressing through the adoption process. Whilst these documents set out the policy direction of travel they are at early stage of the adoption process and so have been considered accordingly.

### Emerging Development Options Review and Relevant Policy Considerations

Having identified the relevant site and area-wide policies we have summarised below those ‘thematic policies’ which have been considered alongside the 3 no. development options prepared by Hunter architects, dated 11th July, as illustrated below.

**Quantum** – In view of the site’s location within the Town Centre and inter alia its proximity to local services and facilities, high density development and indeed intensification of such site’s is actively encouraged by the Council, particularly where this is for residential use. Given this policy imperative, the relatively high density development achieved within the locale and indeed the site as well as the absence of any design or heritage related policies specifically concerning height or quantum of development it is suggested that development of up to 6 storeys could be achieved on site. The initial feasibility studies which assume buildings of 3 storeys in height could therefore comfortably be supported in planning policy terms.

Notwithstanding and assuming additional quantum or alternative uses were required, it might be possible to step the building up to 7 storeys either at the centre of the site (stepping back from main frontage) or on the corner as Alma Road continues and leads to the west.

In terms of the initial development options, it is considered that option 2 represents the preferred approach in respect of planning policy. Option 2 reinforces the street frontage along Alma Road whilst providing an opportunity to create a focal point to the south east corner of the site. If additional quantum is required it might be that the building fronting Alma Road is extended to ‘return’ along the site’s southern boundary whilst respecting the need for vehicular access.

**Design** - The principle of redevelopment of the site is likely to be supported in design terms given the appearance of the existing building which arguably detracts from the appearance of the surrounding area. Detailed design is likely to be subject to discussion but given the relatively ‘bland’ architectural style within the locale it is considered that there is an opportunity to create a scheme with its own identity.

**Parking & Highways** - Relevant planning policy requires 1 vehicular space per 100 sq.m of commercial floorspace. There is no standard for police stations but we would assume the commercial figure as a starting point. In addition, 2% of all car parking spaces should be provided for visitors and there should be 1 motorcycle space for every 20 vehicular spaces. Cycling parking should be provided at 1 space per 20 employees. It is suggested that the current parking analysis scheme (above) be revisited in light of these requirements. The above standards assume the Council agrees that the site is classified as being within a highly sustainable location. .

### Conclusion

The principle of the replacement police station is supported by relevant planning policy and by virtue of the site’s lawful use. Replacement office floorspace is also likely to be supported given the site’s relatively close proximity to the Town Centre and the recent approval of the office scheme immediately to the west of the site. It may however be necessary to consider and discount alternative offices locations within the Town Centre as this is the area of focus contained within emerging, but not yet adopted, planning policy.

Based on our initial review of relevant planning policy and the emerging development options it is our opinion that option 2 represents a logical starting point for any scheme forthcoming. This is primarily on the basis that this scheme reinforces the built frontage along Alma Road.

If the replacement police station and Council’s offices are not progressed it is our opinion that the site is progressed for a residential scheme and promoted accordingly as draft planning policy progresses through the adoption process.

# 5.0 Overview of Development Sites – SITE 3

## DESIGN DEVELOPMENT

On the basis of the merits of each option and the outline planning review, Option 2 has been developed further to determine a ground floor footprint and the quantum of parking that could actually be achieved on this site.

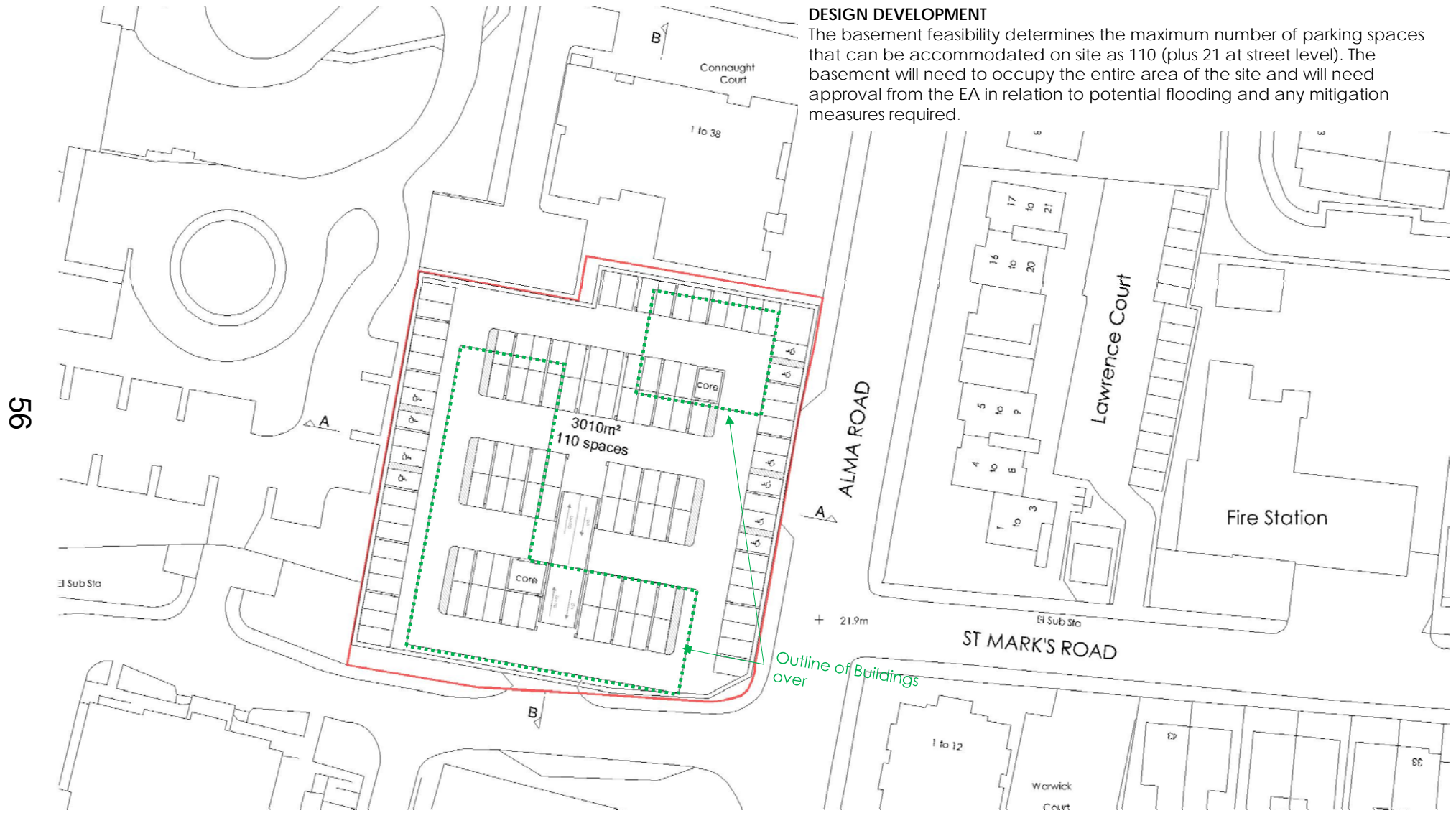


Proposed Ground Floor Arrangement

# 5.0 Overview of Development Sites – SITE 3

## DESIGN DEVELOPMENT

The basement feasibility determines the maximum number of parking spaces that can be accommodated on site as 110 (plus 21 at street level). The basement will need to occupy the entire area of the site and will need approval from the EA in relation to potential flooding and any mitigation measures required.



Proposed Basement Arrangement



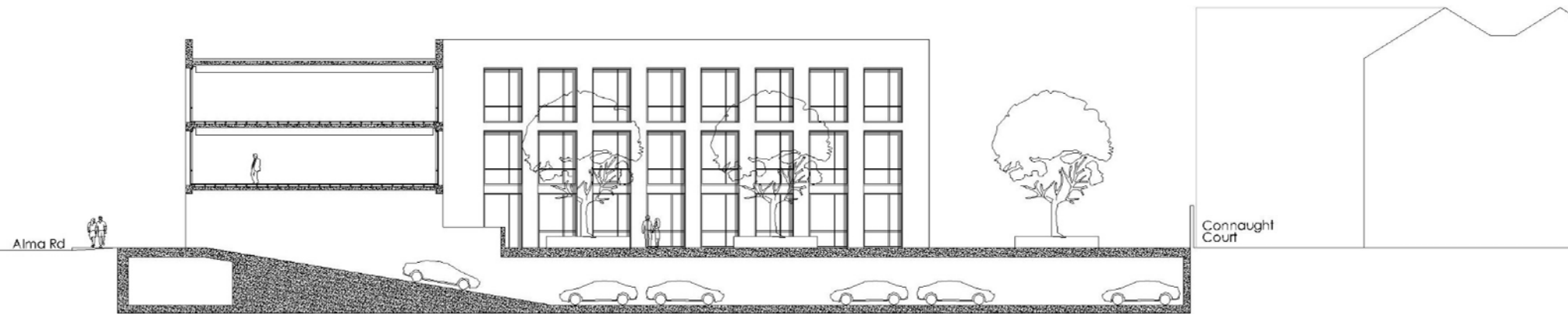
# 5.0 Overview of Development Sites – SITE 3

## DESIGN DEVELOPMENT

The two site sections presented below demonstrate the basement and building heights proposed.



Section A - A (East - West)



Section B - B (North - South)

**SUMMARY**

This site is large enough to accommodate both the TVP and RBWM buildings. The development principle is already established by the presence of the existing police station. The built footprint of the proposed development is substantially greater than that currently occupying the site which results in the need to provide the majority of car parking at basement level. The site location, although fairly central to Windsor, does not offer the “town centre” presence which is essential to both organisations. The current building height at 6 storeys establishes a development mass greater than that needed for this development.

**CONCLUSION**

The costs associated with the construction of the basement and associated waterproofing / Flood alleviation present a real obstacle to the viability of this site. Whilst the site is close to the centre of town it cannot be considered as “town centre” and as such does not meet one of the key requirements of the brief. An additional town centre front counter for both operations could arguably be required in addition to these facilities.

Although presenting many benefits, this site is not the favoured development option for the reasons listed above.

The site is located within a “high value” residential area of Windsor and as such could generate a substantial value if sold subject to planning consent being granted. Although the potential of this site for residential development has not been explored at this stage, it represents a valid development opportunity.

**No further development options have been considered on this site.**

Pros	Cons
A relatively central location	A full basement is required to accommodate parking
Limited planning risk for proposed development	The immediate area is known to present a flood risk below ground level. A wider, all encompassing civil engineering strategy will be required to justify/mitigate potential floodwater displacement .
The site is flat	The site is not “town centre”
The site is easily accessible by car	Even with a full basement the site cannot provide the full parking provision required by the brief.
The site presents an opportunity for a strong street frontage presence for both organisations	The increase in parking on this site will be strongly objected to by local residents
The required building accommodation can comfortably fit on the site	Temporary accommodation and parking will be required off site by TVP during construction to provide a continued responsive service within Windsor.

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SITE 4

York House  
Sheet Street

**SITE 4 – YORK HOUSE, SHEET STREET, WINDSOR**

This site is the current location of the RBWM council offices in Windsor. Sheet Street falls away from the town centre to the south. The “ground” level of the existing site is elevated approximately 1.5m above street level to accommodate undercroft parking below which utilises the natural slope of the site from west to east. Vehicular access is from Sheet Street with a ramped access road to the south of the site.

The existing building is two full storeys with a steep pitched roof which represents a three storey development in total. The accommodation is arranged in a “C” shape to the north of the site but the internal arrangements are inefficient and have resulted in some awkward office spaces and do not present the best working environment.

To the south of the site, 3 storey terraced town houses are set back from the pavement with front gardens. York House represents the “beginning of the high street” as it is the first non-residential building along this road leading into the town and the high street with all of its shops and cafes. York House itself does not have any strong architectural merit and sits rather uncomfortably within the street scape.

Parkside House to the north of this site is a full 3 storeys in height and provides office accommodation onto Sheet Street. Regent Court behind provides 3 full storeys of residential accommodation.

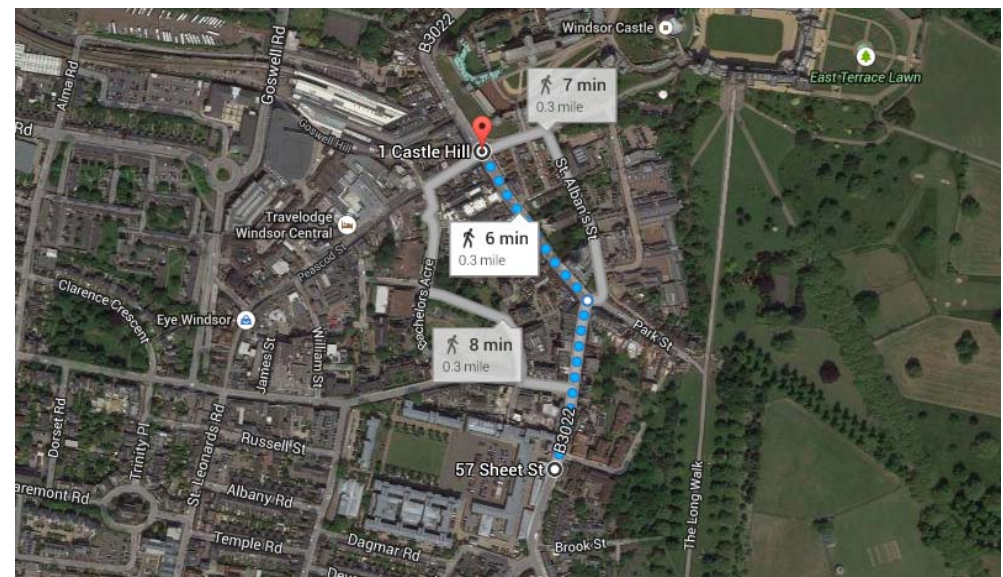
Victoria Barracks faces the site on the opposite side of Sheet Street to the West and presents a 1.5 to 2 storey wall onto the back edge of the pavement.

The Long Walk leading up to Windsor Castle sits directly behind the site to the East.

The site is located 0.3miles from the town centre and is an easy 6 minute walk on foot.



Birds Eye View of Site



Site Location in relation to “town centre”



York House from the North along Sheet Street



York House from the South looking along Sheet Street



Existing 3 storey terraced housing to the south



Parkside House to the north of the site

61



Victoria Barracks opposite York House



Vehicular Access into York House car park to the south

62



Narrow secondary access along Brook Street to the south



Level change from Sheet Street into undercroft car park

# 5.0 Overview of Development Sites – SITE 4



Pedestrian right of way from York House to The Long Walk



Third party parking spaces access from Brook Street



Undercroft car parking spaces



Grade level overflow parking spaces



Southern boundary with residential accommodation behind

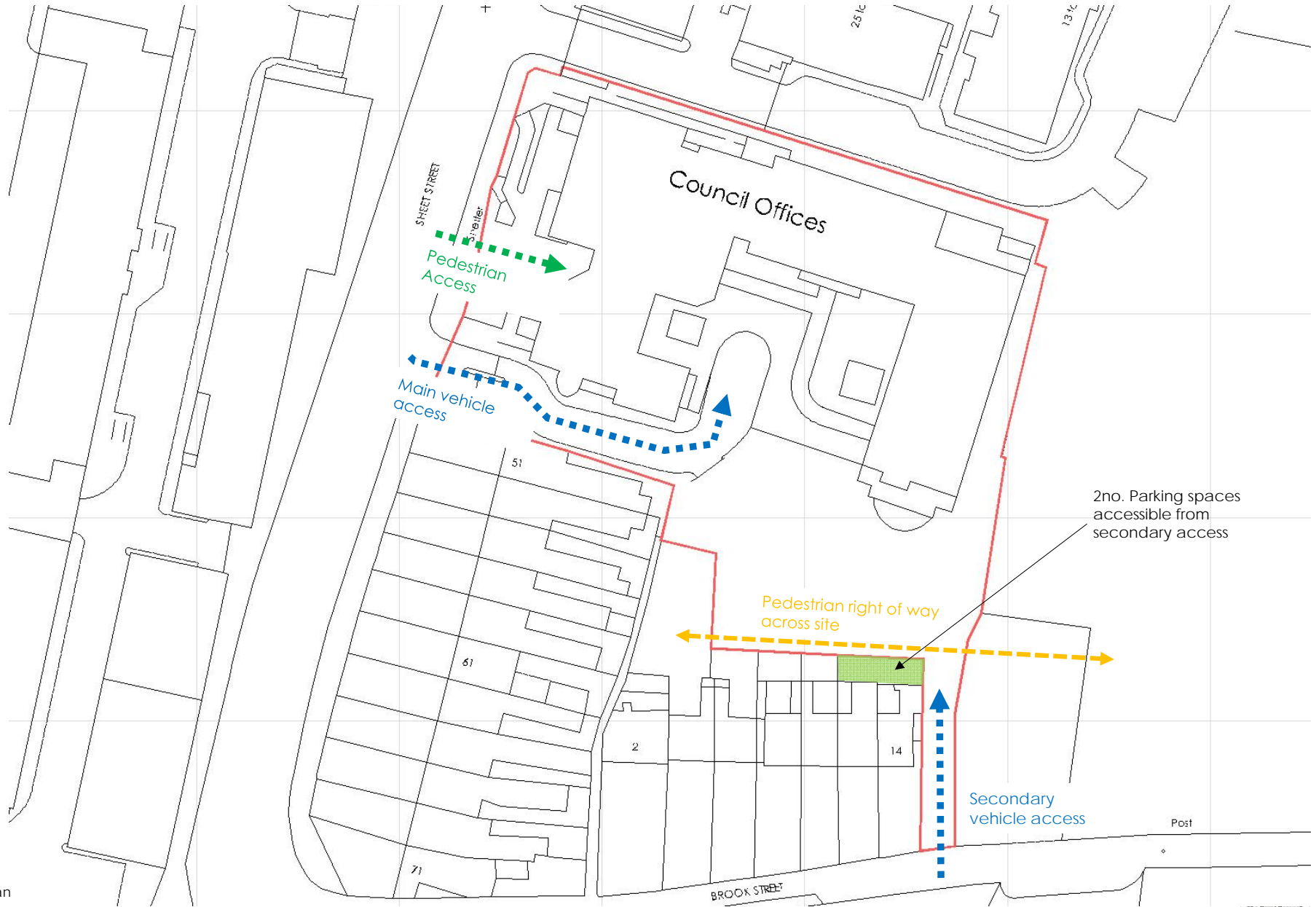


Secondary access onto Brook Street

63

# 5.0 Overview of Development Sites – SITE 4

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Existing Site Plan



## PARKING

The site falls from West to East which lends itself to the natural creation of the undercroft car park area. The diagram below demonstrates that the majority of the car parking required by the brief can be accommodated on a single level within the boundary of the site. Any shortfall can be accommodated on the podium level at ground floor level



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## 5.0 Overview of Development Sites – SITE 4

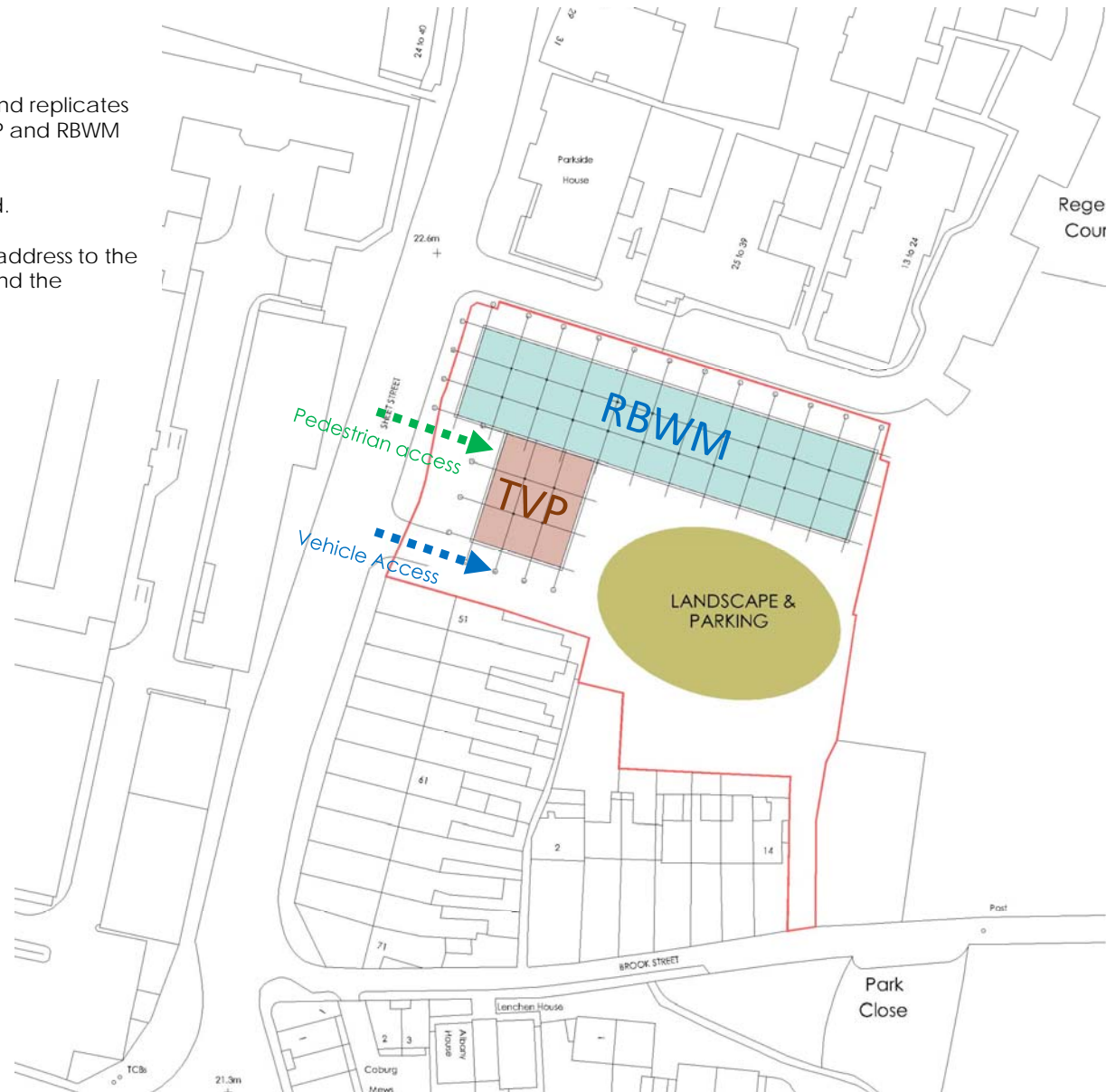
### OPTION 1

This option creates a strong street frontage along Sheet Street and replicates the built form of the existing building along this façade. Both TVP and RBWM share an equal proportion of street frontage.

Pedestrian and vehicular accesses remain as currently arranged.

The juxtaposition of the two blocks provided the opportunity to address to the two street frontages presented by Parkside house to the north and the residential terrace to the south.

Landscape and parking are tucked away to the rear of the site.



Redevelopment Option 1

## 5.0 Overview of Development Sites – SITE 4

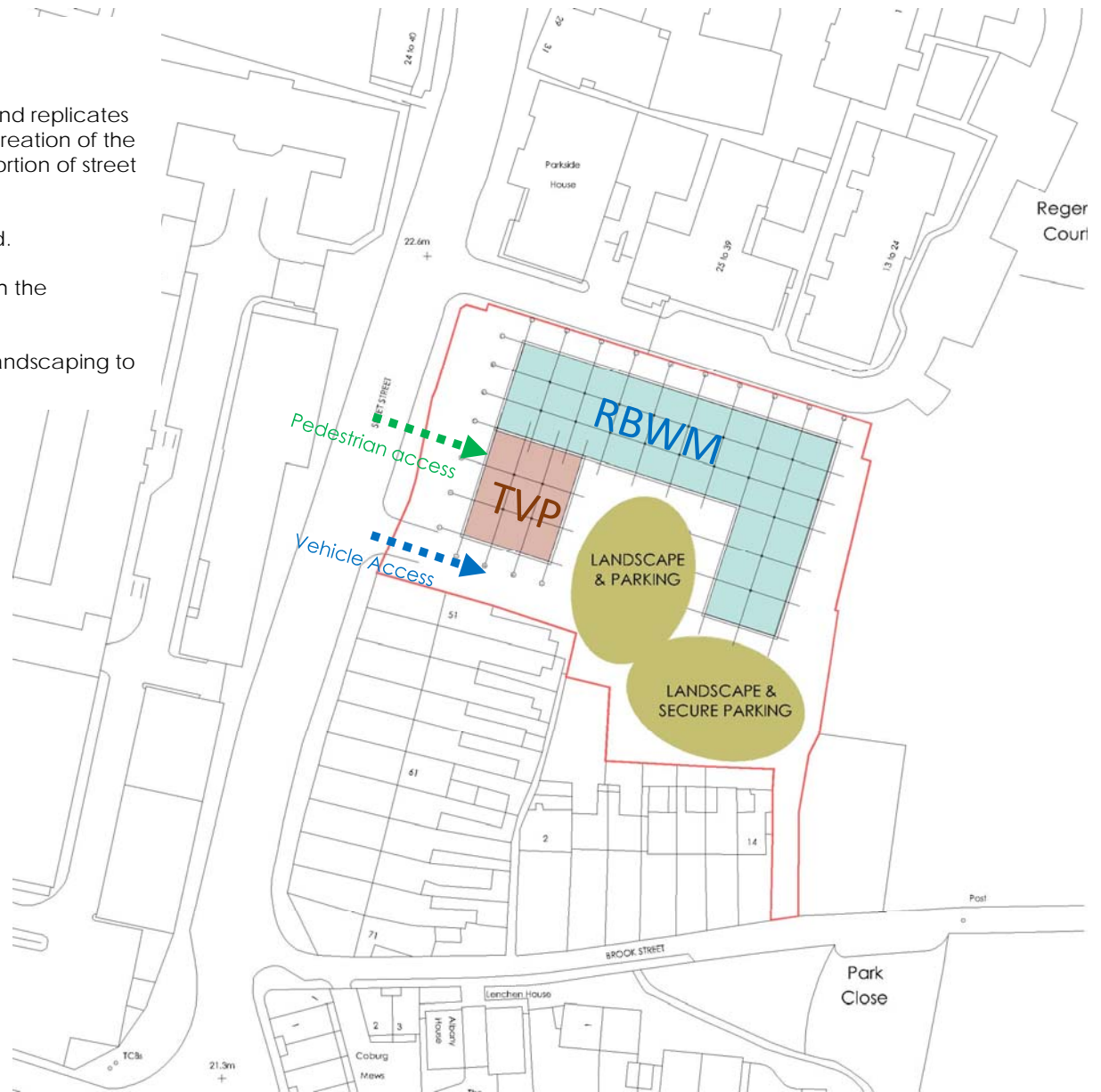
### OPTION 2

This option creates a strong street frontage along Sheet Street and replicates the built form of the existing building for the entire site with the creation of the "C" shape plan form. Both TVP and RBWM share an equal proportion of street frontage.

Pedestrian and vehicular accesses remain as currently arranged.

The building line onto Sheet Street is set back to line through with the residential terrace to the south.

A central courtyard is created to accommodate parking and landscaping to create a setting for office workers.



Redevelopment Option 2

## 5.0 Overview of Development Sites – SITE 4

### OPTION 3

This option breaks the street frontage but creates an inviting courtyard frontage to the development. This helps to push the mass of the development to the rear of the site and creates a sense of “arrival” for visitors.

The development area is also pulled away from the existing residential Terrace to the south and exploits views over the trees to the East towards the Long Walk and beyond.

Both buildings face onto the central courtyard area.

Pedestrian and vehicular accesses remain as currently arranged.



Redevelopment Option 3

## PLANNING APPRAISAL

### Executive summary

- The site is located within Windsor Town Centre whereby a range of uses are considered acceptable in principle. This includes the provision of public/community services such as the proposed police station. The re-provision of the Council's offices would be acceptable by virtue of the site's current lawful use (B1a) and its location within a designated Town Centre.
- The perceived or actual impact of the operation of the police station on the amenities of neighbouring residential properties will need to be carefully considered either through its siting, detailed design measures or the use of conditions.
- Similarly the design aspect of any redevelopment proposal will need to be carefully considered in the context of the site's location within the Town Centre Conservation Area, the statutorily listed terrace of residential properties to the south of the site and Green Belt to the east.
- The heritage constraints associated with the site are such that it is considered three storeys would be an acceptable height for any replacement scheme, albeit an increase to four storeys could be possible on the basis of the site's scale, its relatively isolated nature and operational requirements.
- Consultation with stakeholders and the public is likely to be key to the prospects of success of any scheme forthcoming. For example it will be important to fully articulate the operational requirements of the police station which have determined this site is likely to be an appropriate location.
- The initial feasibility studies represent a logical starting position for any replacement proposal. **Based on the constraints and policy considerations identified within this appraisal we would recommend either option 1 or 3 is progressed if this site is selected as the preferred option. This is primarily on the basis of the majority of development being sited away from the adjacent Listed Buildings and Green Belt**
- **The planning risk associated with the redevelopment of the site for replacement Council offices and a new police station, as envisaged through options 1 and 3, is considered to be low**

### Site description

The site measures 0.37 ha and is located to the east of Sheet Street which is within the Windsor Town Centre boundary.

Immediately opposite the site is an imposing brick wall (circa 4 meters) that forms part of the curtilage to the Victoria Barracks. The site's northern and southern boundaries are bound by two vehicular points of access with three storey office and residential properties immediately beyond. Those residential properties to the south of the site (51-77 Sheet Street) are Grade II listed. The site is located within the wider Windsor Town Centre Conservation Area. Land to the rear of the site is undeveloped and designated as Green Belt land under relevant planning policy.

It is understood that the site is currently occupied by Royal Borough of Windsor and Maidenhead Council for office purposes (Use Class B1a). The existing property is two storeys in height with a high pitched roof, albeit taking into account steps to the front of the property and the ramped access the overall height is the equivalent to 3 storeys.

The site is well served by local services and facilities by virtue of its location within Windsor Town Centre and a bus stop immediately outside which provides regular services to the wider area.

### Planning history

Our review of the Council's online planning register has identified a number of small scale applications relating to the buildings lawful use for offices (Use Class B1a). There are no applications considered to be of relevance to the redevelopment of the site. However, for the sake of completeness those applications identified are summarised below.

- 97/76198/FULL - Proposal Erection of single storey extension to reception with glass covered walkway within courtyard - Permitted 19/09/1997.
- 97/75337/REG3 - Proposal Erection of a two storey rear extension to provide 675 square metres of additional office accommodation with adjacent car park - Permitted 12/05/1997; and
- 97/75336/CAC - Proposal Demolition of squash court building and double garage - Permitted 1/05/1997.

## PLANNING APPRAISAL

### Planning Policy Context

The redevelopment of the site is subject to those policies contained within the adopted Development Plan for The Royal Borough of Windsor and Maidenhead which comprises the following documents:

- The Local Plan (Incorporating Alterations) – Adopted in June 2003 (saved policies).
- The Maidenhead Town Centre Area Action Plan

The above-mentioned documents are supplemented by further local level guidance in the form of SPD's and SPG's which we refer to as necessary throughout the following commentary. Additionally, 'material considerations' are also to be accounted for as part of the determination process. This includes the National Planning Policy Framework (2012) and associated National Planning Practice Guidance (2014).

### Emerging Local Plan

In addition to the above emerging planning policy in the form of an updated Borough-wide Plan, Site Allocations DPD and Neighbourhood plan is progressing through the adoption process. Whilst these documents set out the policy direction of travel they are at early stage of the adoption process and so have been considered accordingly.

### Site & Area Specific Planning Policy

The following area and site specific policy designations apply to the site and its future redevelopment.

**Windsor Town Centre** – There is an overall policy presumption in favour of the redevelopment of site's within the Town Centre for a range of uses including those relating to community/public services. Such proposals will be expected to contribute towards the historic and architectural character of the Town Centre. Town Centre sites are considered to represent an important supply of land through intensification, subject to design and heritage considerations.

**Windsor Town Centre Conservation Area & Listed Buildings** – Development proposals located within this Conservation Area will be required to enhance or preserve the character or appearance of the area. Similarly, any replacement proposal will need to consider any impact on the setting of those listed buildings to the south of the site (51 to 77 Sheet Street). Please note that the Conservation Area Statement is not currently available but will need to be reviewed.

**Green Belt** – The site is not located within the Green Belt but is immediately adjacent to it. Any redevelopment proposal will therefore need to account for potential impact to setting by virtue of proximity and height. A detailed review of the Green Belt and the boundary of the site will establish how the scheme may or may not need to address this issue, albeit from initial investigations the extent of boundary planting between the site and the Green Belt is such that this is not likely to be a key issue.

**Flood Risk** – For the avoidance of doubt the Environment Agency has confirmed that the site is not located within a Flood Risk Zone.

Relevant policies include:

- Policy DG1 (Design Guidelines)
- Policy CA1 (Development in Conservation Areas)
- Policy CA2 (Development affecting Conservation Areas)
- Policy H3 (Affordable housing within urban areas)
- Policy H6 (Town Centre Housing)
- Policy H10 (Housing layout and design)
- Policy WTC3 (Town centre strategy)
- Policy WTC1 (Town centre strategy)
- Policy NAP3 (Polluting Development)
- Policy T5 (New Developments and Highway Design)
- Policy P4 (Parking within Development)
- Adopted Parking Standards

## PLANNING APPRAISAL

### Emerging Development Options Review and Relevant Policy Considerations

Having identified the relevant site and area-wide policies we have summarised below those 'thematic policies' which have been considered alongside the 3 no. development options prepared by Hunter architects, dated 11th July and illustrated overleaf.

**Quantum** – In view of the site's location within the Town Centre and inter alia its proximity to local services and facilities, high density development and indeed intensification of such site's is encouraged by the Council, particularly where this is for residential use. It is however our opinion that in the instant case the amount of development that can be achieved through the redevelopment proposal will be very much design-led, taking into account the various heritage assets that exist (Conservation Area and Listed Buildings). The initial feasibility studies which assume buildings of 3 storeys in height represent a realistic starting point, primarily on the basis of the height of the existing building on site and those located adjacent.

Notwithstanding, the scale and relative isolated nature of the site is such that it might be possible to step the building up to 4 storeys either at the centre of the site (stepping back from main frontage) or on the corner between Sheet and York Street so as to create a focal point to the new development. This would strike a balance between policy supporting intensification and that seeking to safeguard heritage assets.

In terms of the initial development options, it is considered that options 1 and 3 represent the preferred approach in respect of planning policy. Option 1 reinforces the street frontage whilst stepping forward at the corner at York and Sheet Street which creates a focal point that is importantly furthest away from the listed buildings to the south of the site. A suitable gap between the built elements of this scheme and adjacent listed buildings has been incorporated which will help address any issues of perceived impact to the setting of these properties. It is helpful that the majority of development is orientated towards the office building to the north of the site. Not only does this avoid development close to the listed buildings to the south but it will also reduce impacts associated with the operation of the police station on existing residential amenities.

Option 3 also orientates the majority of development to the north of the site which is important given the listed buildings to the south (design and amenity). It is however considered that there may be a potential impact to the Green Belt by virtue of the fact that the building 'returns' along the eastern boundary which is adjacent to the Green Belt. It is not however possible to say at this stage the level of impact given that significant boundary planting exists. Notwithstanding, by extending the built envelope along the eastern boundary it becomes closer to the adjacent residential properties which may give rise to amenity-related issues, such as overlooking.

Option 2 creates a building that is within close proximity to both the Green Belt and listed buildings to the south of the site. It appears the most intensive use of the site when compared to the other options and for these reasons represents the least preferred approach.

**Design** - The principle of redevelopment of the site is likely to be supported in design terms given the appearance of the existing building. Detailed design is likely to be subject to negotiation with the Council and statutory consultees such as English Heritage. Whilst the detailed aspects of the scheme are to be evolved it is suggested that design cues could be taken from the adjacent terrace of residential properties given their architectural quality. A review of the Town Centre Conservation Area Statement will be critical to detailed design.

**Parking & Highways** - Relevant planning policy requires 1 vehicular space per 100 sq.m of commercial floorspace. There is no standard for police stations but we would assume the commercial figure as a starting point. In addition, 2% of all car parking spaces should be provided for visitors and there should be 1 motorcycle space for every 20 vehicular spaces. Cycling parking should be provided at 1 space per 20 employees. It is suggested that the current parking analysis scheme (above) be revisited in light of these requirements. The above standards assume the Council agrees that the site is classified as being within a highly sustainable location.

## PLANNING APPRAISAL

It is noted that an area between the site and adjacent terrace of residential properties has been allocated for parking and landscaping. Consideration will need to be given to the incorporation of a service area for deliveries. Turning heads and areas for refuse for example will need to be investigated further and to this end it is recommended that a Transport Consultant is engaged.

### Conclusion

The principle of the replacement office use and police station is supported by relevant planning policy. The key consideration in this respect is potential impact of the operation of the police station on adjacent residential uses (perceived or actual). Detailed design, siting of building and the use of conditions will help address these issues.

Based on our initial review of relevant planning policy and the emerging development options it is our opinion that option 1 and 3 represents a logical starting point for any scheme forthcoming. This is primarily on the basis of the majority of development being sited away from the adjacent Listed Buildings and Green Belt. It is considered that an argument could be advanced to increase part of the building to 4 storeys which may serve to reduce footprint and increase the overall area available for car parking which appears difficult to accommodate currently.

## SUMMARY

The overall site area of the York House site is 3,704m<sup>2</sup>. The site is ideally located for access from the town centre and the principle of development is established by the current site uses and amount of parking already in existence.

The split level nature of the existing site lends itself to a natural undercroft for parking.

The full quantum of development can be accommodated .

Pros	Cons
Good sized site which can accommodate all required buildings and parking	The site has projected high values for redevelopment
Little planning risk for proposed development in terms of principle of development	The site is adjacent to the long walk and listed buildings to the south. The ultimate design will need to respond to these aspects accordingly
The sloping nature of the site can accommodate parking without excessive excavation costs	The public car park will be lost during construction
The site is ideally located close to the town centre	RBWM will require temporary office facilities during construction – assumed within the under utilised TVP office at Alma Road
The secondary access onto Brook Street creates an easy emergency for TVP	

## CONCLUSION

This site provides the ideal location for the redevelopment of both facilities in terms of location and size.

Further design development has been undertaken to determine a more accurate development potential.

**More detailed proposals for this site follow in this report.**

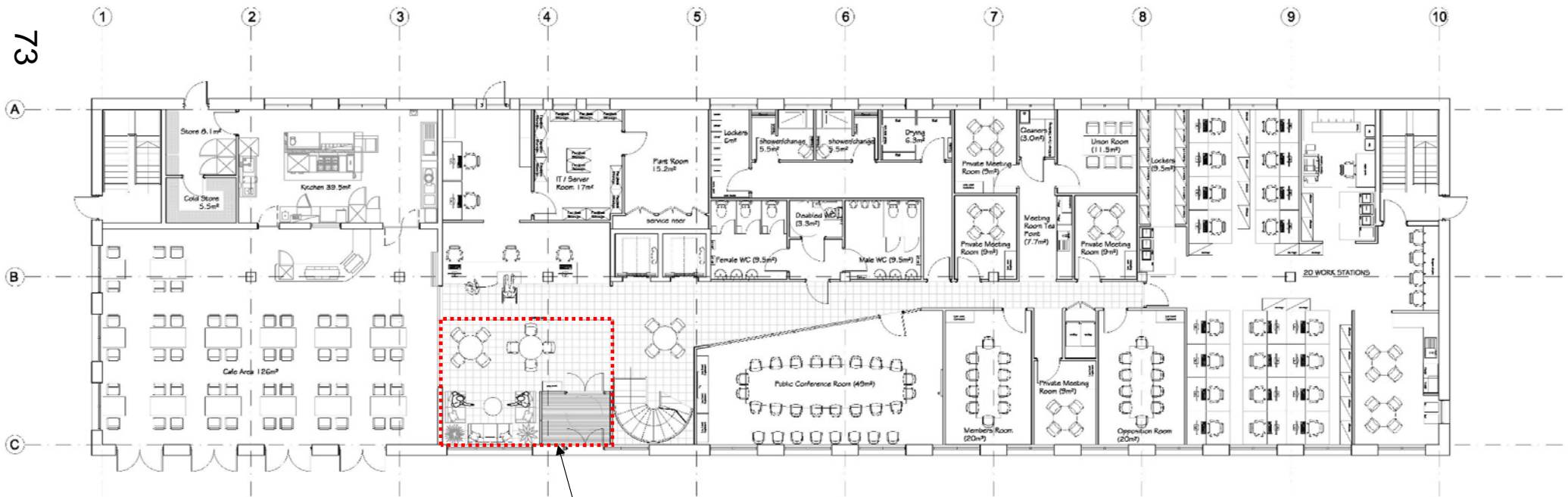


## DESIGN DEVELOPMENT - BUILDINGS

On the basis of the merits of each option and the outline planning review, Option 3 has been developed further on the York House site (site 4) to determine an accurate development potential for the site. Both the Police Station and the RBWM office have been designed through to full feasibility stage. Whilst these drawings do not represent the full and final proposal for this site they provide confidence that the site can deliver the required quantum of development.

The GIA of the RBWM building is - **2,343m<sup>2</sup>**  
 The GIA of the TVP building is - **756m<sup>2</sup> (3 stories) 504m<sup>2</sup> (2 stories)**

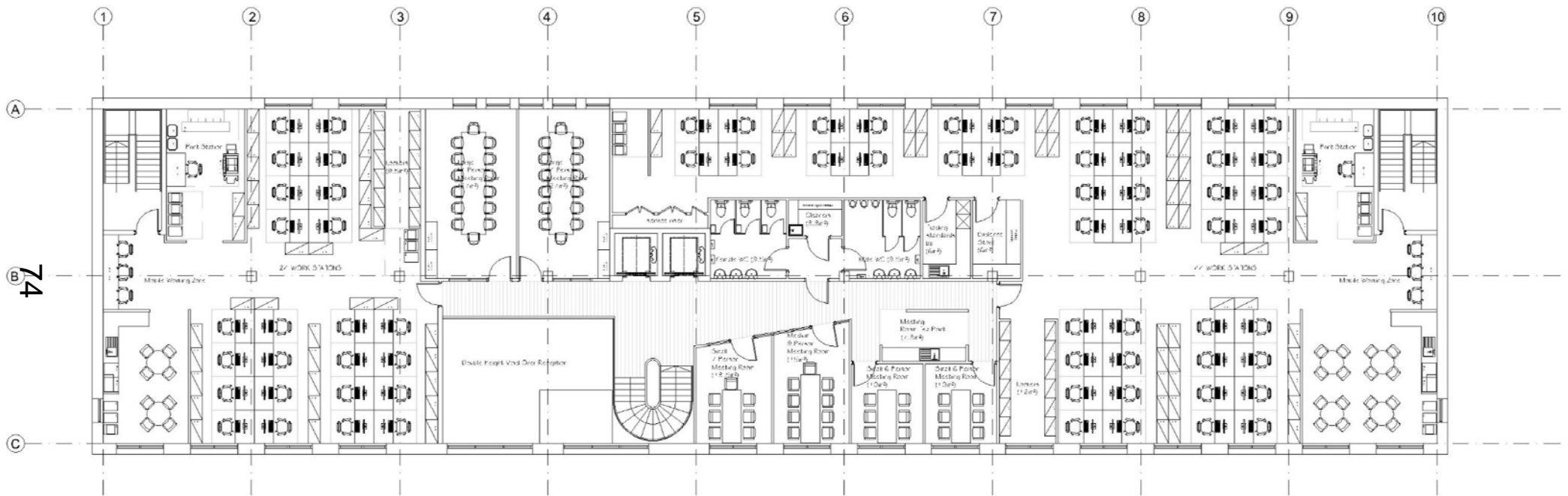
## RBWM BUILDING



RBWM Proposed Ground Floor Arrangement

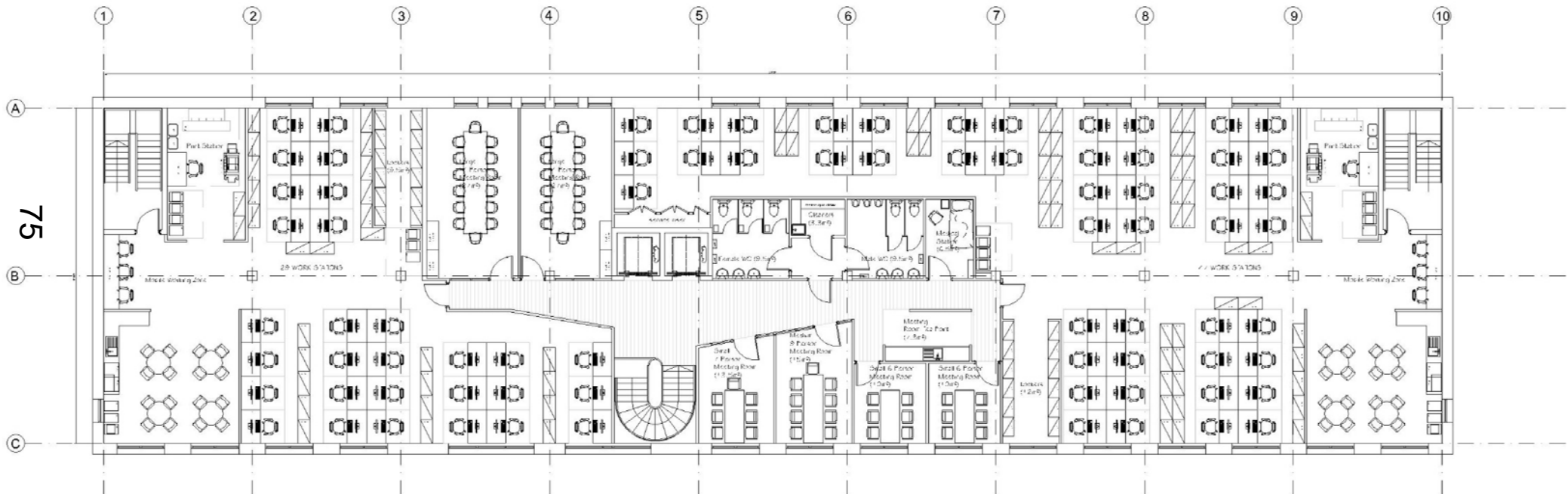
Double height space created to entrance area

# RBWM BUILDING



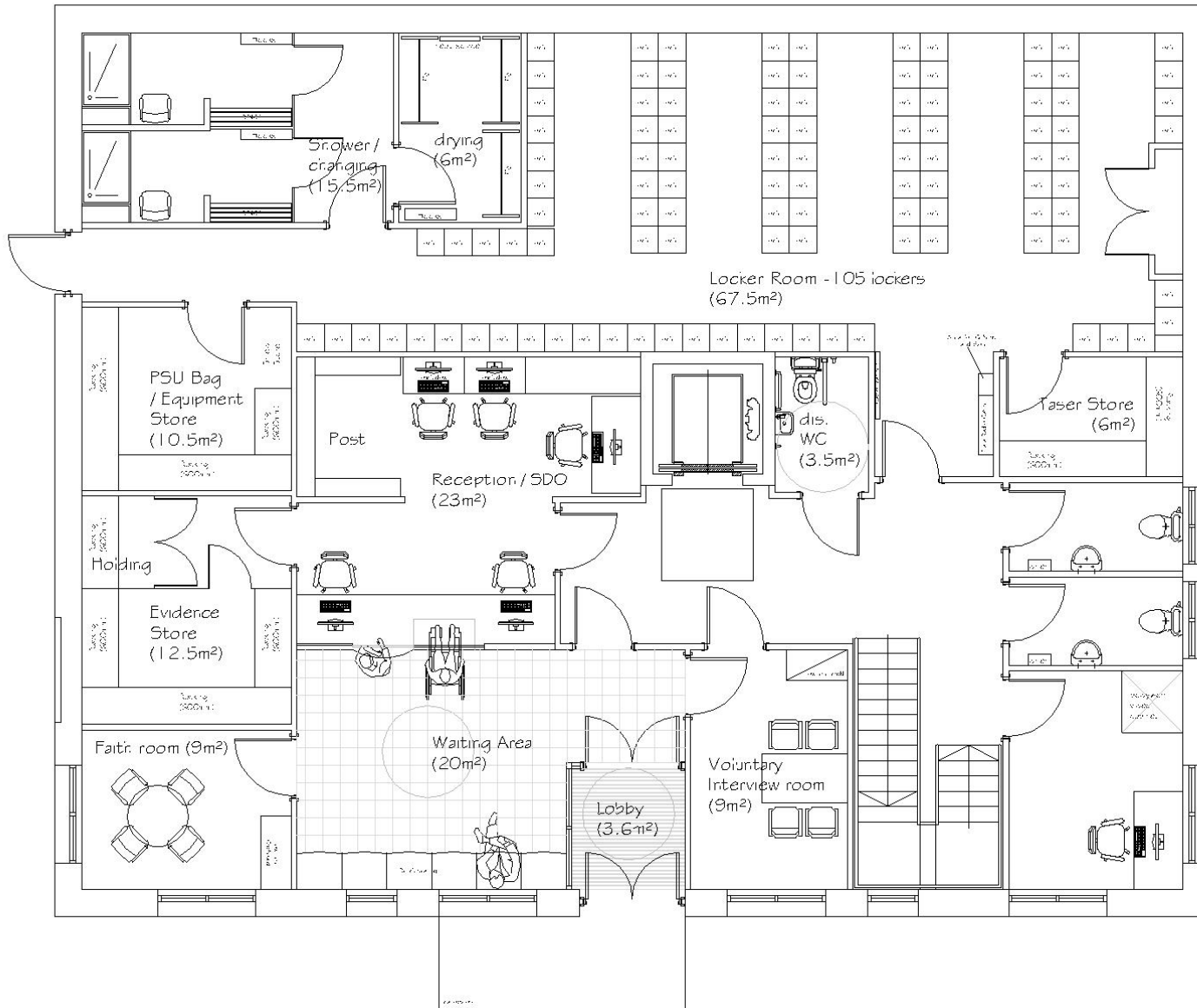
RBWM Proposed First Floor Arrangement

# RBWM BUILDING



RBWM Proposed Second Floor Arrangement

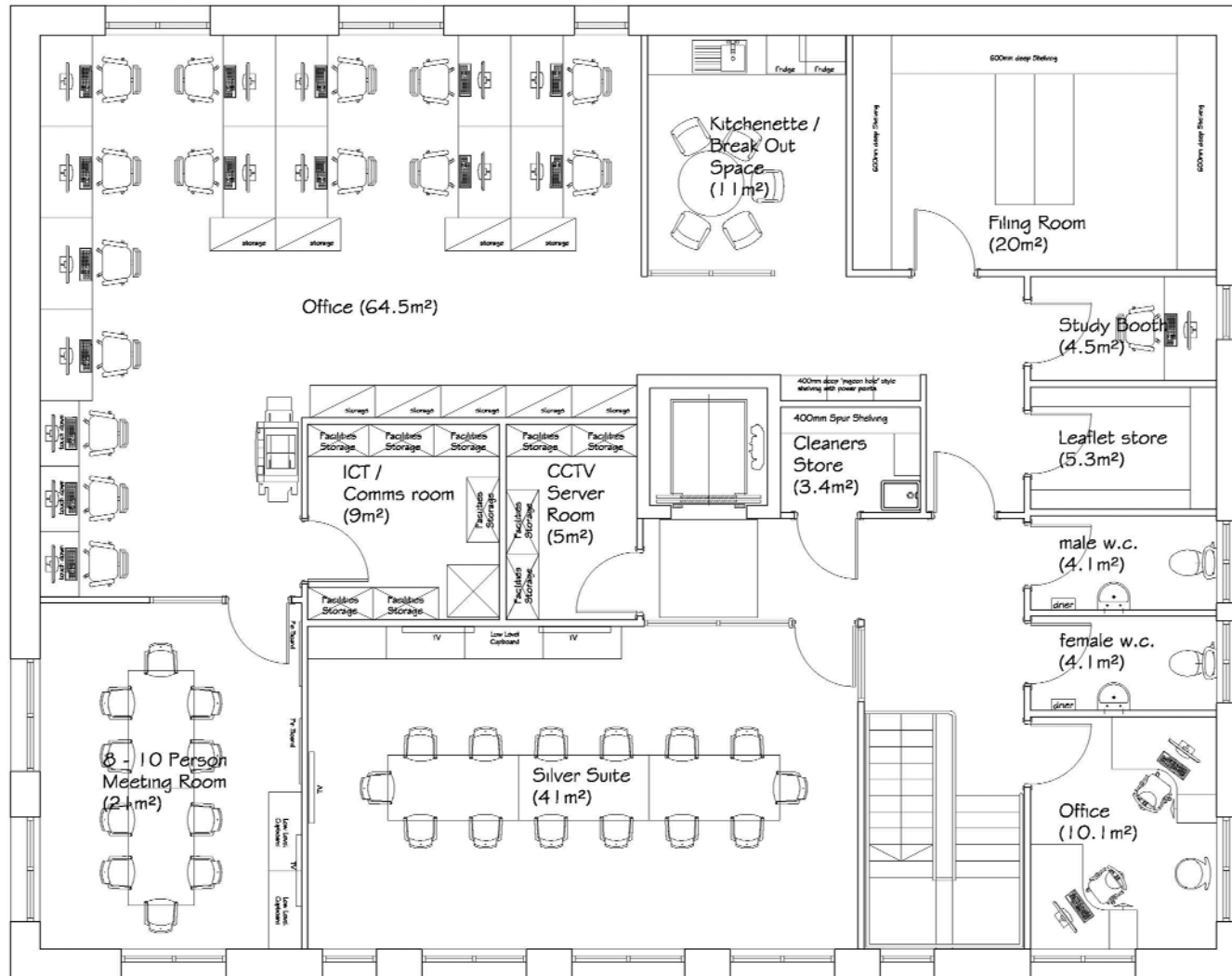
# TVP BUILDING



TVP Proposed Ground Floor Arrangement

76

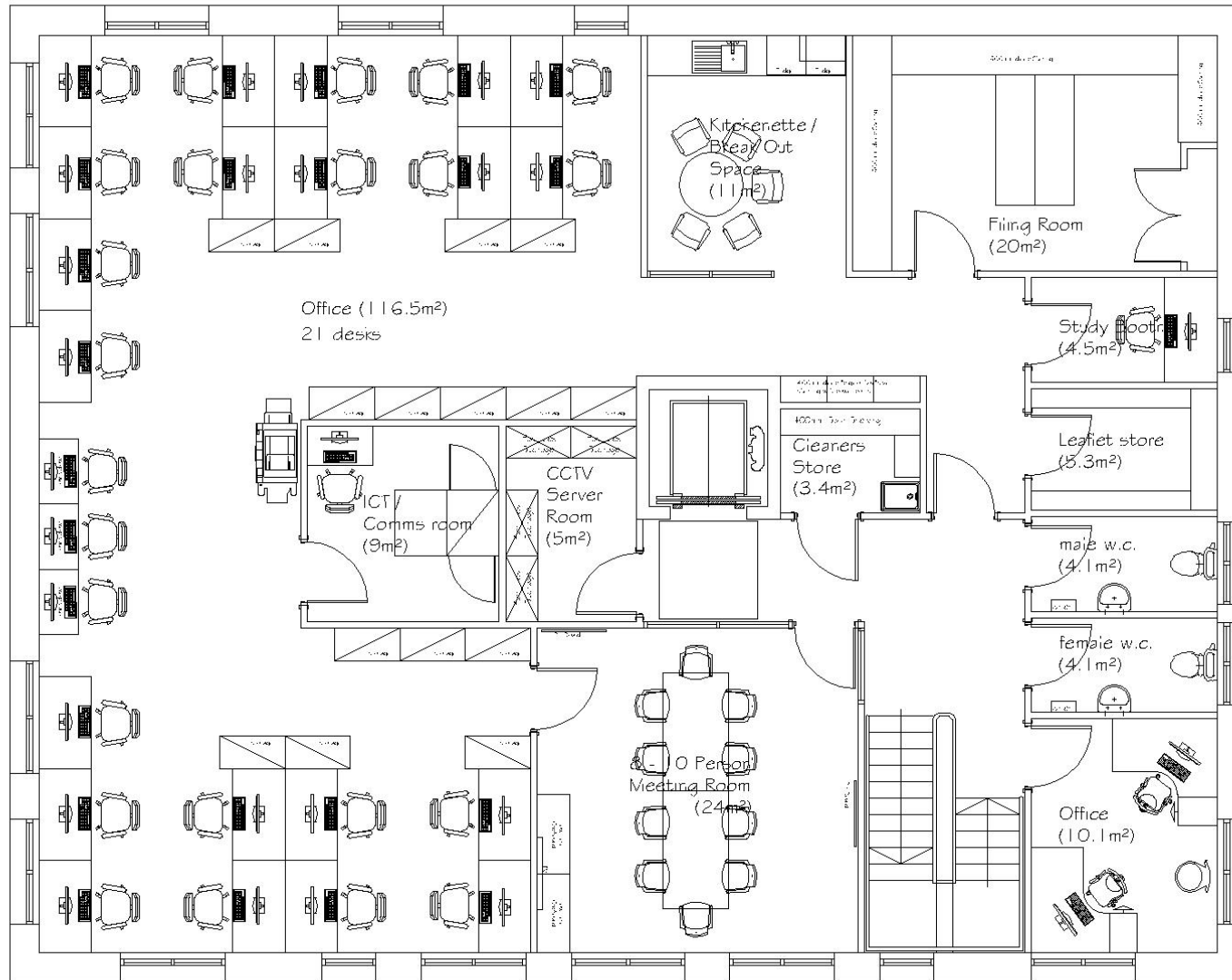
# TVP BUILDING



TVP Proposed First Floor Arrangement (2 Storey Option)

77

# TVP BUILDING

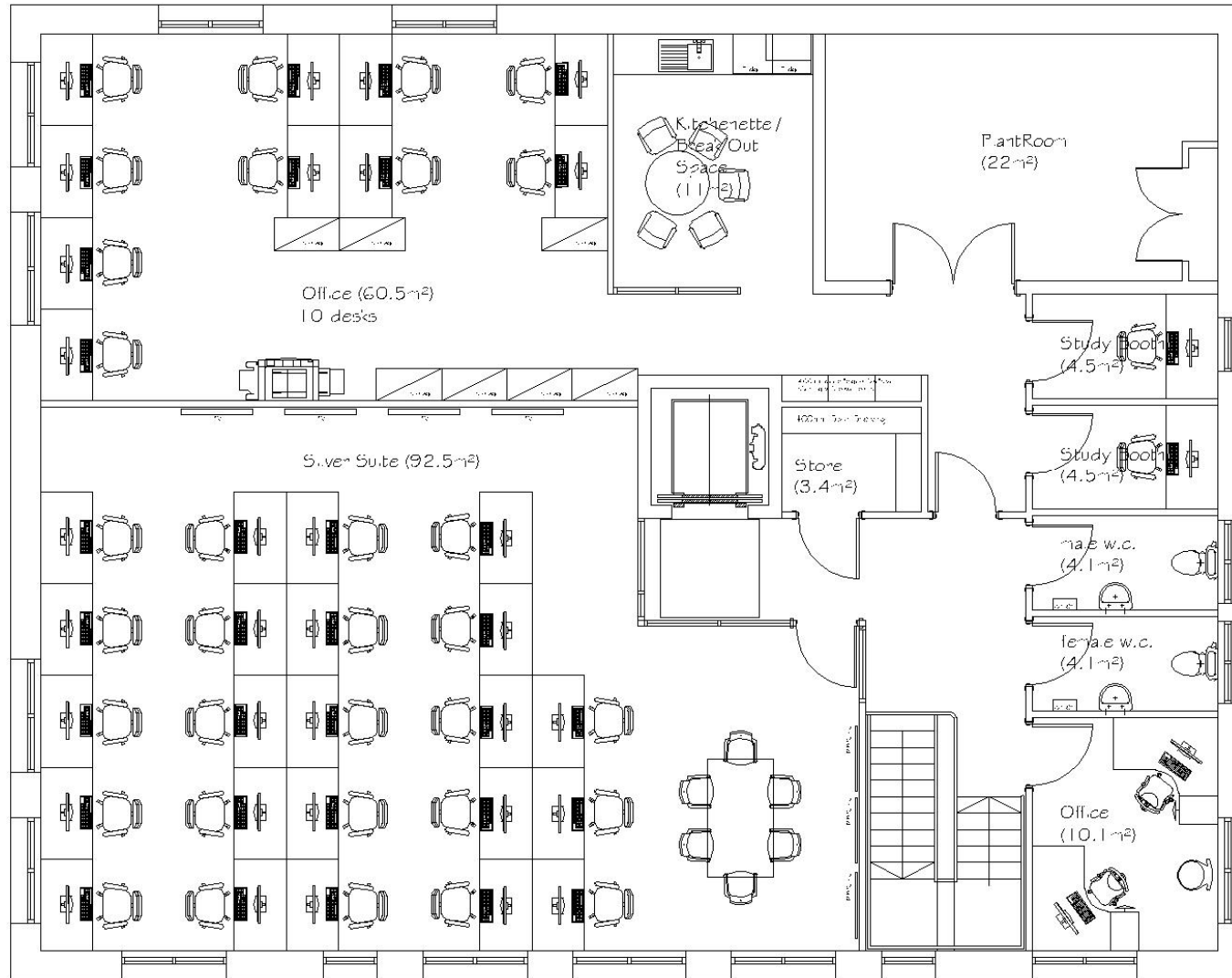


TVP Proposed First Floor Arrangement (3 storey option)

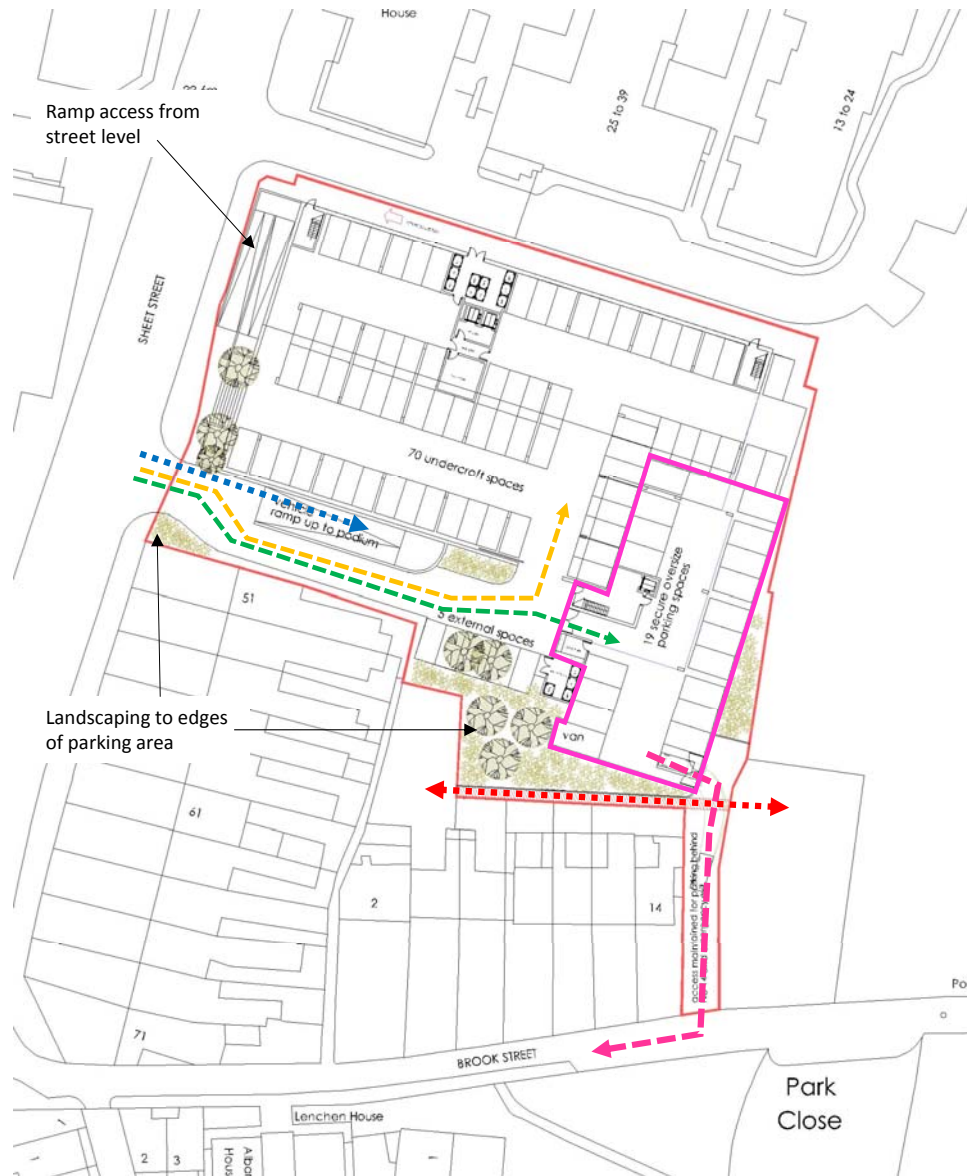
78

# TVP BUILDING

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TVP Proposed Second Floor Arrangement (3 storey Option)



### DESIGN DEVELOPMENT - ACCESS

The Sheet Street Site has been developed to maximise the use of the natural fall across the site from West to East.

Vehicles enter the site from Sheet Street and fall down the general site level and turn left into the car park sitting below the transfer slab just above street level. A ramp up to the podium level provides access to additional parking on the landscaped courtyard deck.

The car park holds 70 cars and can be secured via a gate at the point of entry. 5 "overspill spaces are located externally, directly adjacent to the new police station.

Police vehicles continue straight on, into the secure compound sited below the TVP building. 19 oversized parking spaces (including a van space) are provided within the secure compound for police vehicles. Emergency egress can be accommodated via a gate leading onto Brook Street should the exit onto Sheet Street become blocked at any time.

Pedestrians approach the site from sheet street and can take a ramp or a wide staircase up to the podium level.

- Public vehicle access to podium level car park
- Public vehicle access to undercroft car park
- Police access to secure car park
- Emergency exit
- Pedestrian right of way maintained
- Secure Police compound



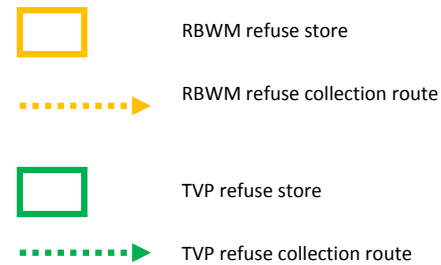


### DESIGN DEVELOPMENT - REFUSE

Two refuse stores are provided at undercroft level for use exclusively by TVP and RBWM. Current refuse collections are daily but stores have been sized to enable weekly collections if required.

The RBWM refuse store will be collected from sheet street via the alleyway to the north of the site. The TVP refuse store will be collected from the new access road into the site.

Both stores have been sized to accommodate standard and recycled waste.





### DESIGN DEVELOPMENT - SETTING

The podium sits approximately 1.5m above street level at the north of the site and provides a landscaped setting for the two civic buildings.

Whilst the majority of the finishes will be hard, careful selection of materials can create a shared surface for pedestrians and vehicles. Where possible, soft planting and street trees will break up the courtyard area. Lockable cycle storage is also provided at strategic locations around this area.

The buildings have been located to create a clear separation between the office buildings and the Grade II listed terrace to the south onto Sheet Street. This configuration also provides the opportunity to create a small scale “civic square” at podium level. The café has been located to have an element of street frontage to creative activity onto the street. A terrace area allows patrons to spill out onto the courtyard area and enliven the space.

The entrance to the RBWM office has been centred on the courtyard and is easily identifiable by visitors. Similarly, the TVP entrance is directly ahead as pedestrians approach.

The short façade facing onto Sheet Street addresses the building line set by the existing Parkside House and presents the opportunity for branding by both organisations.

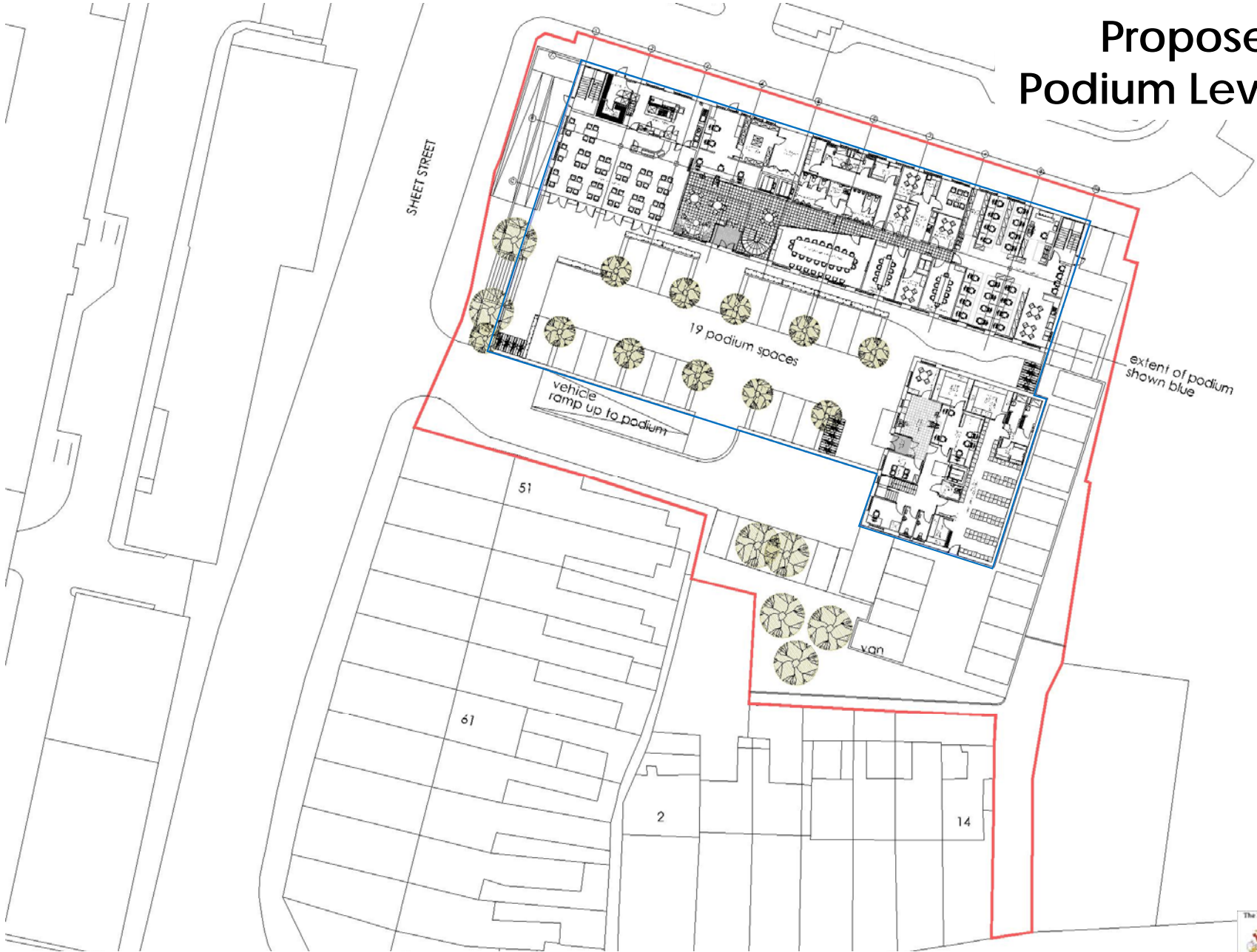
- Pedestrian access to podium level
- Building Entrances
- Café Terrace
- Cycle parking

# Proposed Car Park Level



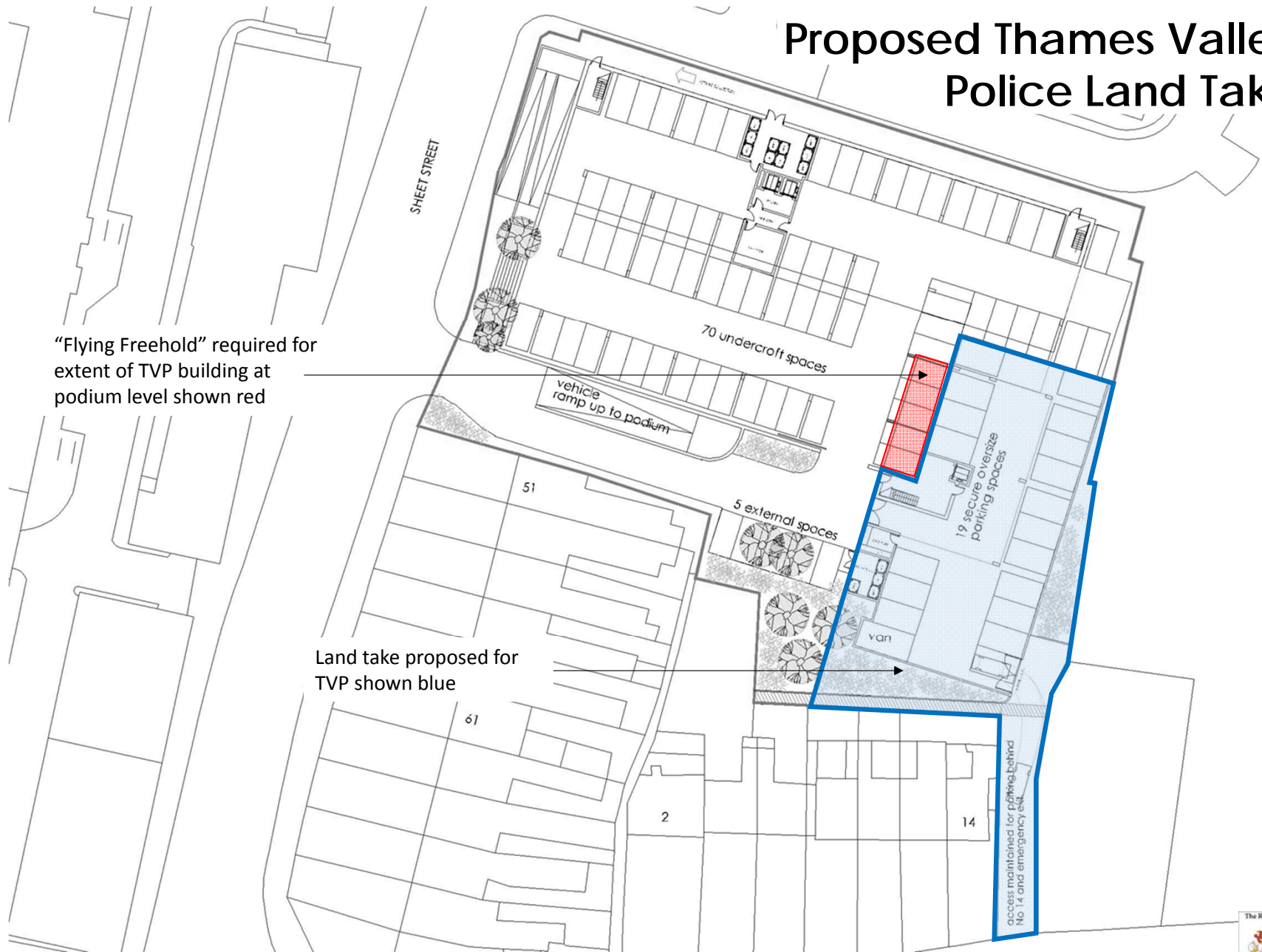
83

# Proposed Podium Level



84

# Proposed Thames Valley Police Land Take



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Site	Appropriate Location?	Appropriate Size?	Planning Risk	Viable Option?
Bachelors Acre Library	Yes	No	Moderate	No
Tinkers Lane Depot	No	Yes	Moderate	No
Alma Road Police Station	No	No	Low	No
York House, Sheet Street	Yes	Yes	Low	Yes

SITE	LOCATION	TOTAL COST	INDICATIVE SPLIT	
			TVP	RBWM
2	Tinkers Lane	£17m to £18m	£3,800,000	£13,400,000
3	Alma Road	£14.5m to £15.5m	£4,400,000	£10,700,000
4	Sheet Street	£12.75m to £13.75m	£3,300,000	£9,600,000

## NOTES:

- All costs based on 3 storey buildings for both TVP and RBWM
- Indicative split of joint costs presently based on building floor areas apart from directly apportioned cost items
- All costs include furniture and equipment, but exclude any new IT equipment
- All costs include allowances for professional fees etc
- Allowances have been made for temporary relocation etc costs as applicable, but further work would be required
- All costs are at current building price levels, with no allowances for inflation
- No allowance for VAT
- No allowances for any land acquisition costs

## SUMMARY

Four sites have been appraised within this report in order to determine their suitability for the redevelopment of shared Thames Valley Police station and the Royal Borough of Windsor & Maidenhead offices.

They have been assessed in relation to their access, size, location and ability to fulfil the brief from both clients.

**Bachelors Acre Library** site has been **dismissed** on the grounds that the site is not large enough to accommodate both buildings, the required car parking and that the vehicular access is too restrictive for the trip generation required for the proposed development.

**Tinkers Lane Depot** has been **dismissed** on the grounds that its physical location is inappropriate for the needs of both Thames Valley Police and The Royal Borough of Windsor & Maidenhead. Although this site is large enough to accommodate the full development requirements, a police station and council office 2.5 miles (or nearly an hours walk) from the town centre are not providing an accessible service to the town's residents. Additional public facing facilities will be required in the town centre for both clients if this option was pursued. A town Centre location is a critical element of the brief for both clients.

**Alma Road Police Station** site has been **dismissed** on the grounds that the site is not large enough to accommodate the required quantum of parking without creating a full basement to the site. The costs of construction are anticipated to be disproportionate to the limited number of spaces that can be accommodated. Whilst this site is fairly central, it is still not within the central "hub" with other shops and amenities and is a more residential area. It is recommended that this site is sold for residential development.

**York House, Sheet Street** is large enough to accommodate the requirements of the brief including the required building footprints and car parking. The site is ideally located in the centre of town and is only a short walk for pedestrian visitors. **Our recommendation is that this site option is developed further** to fully understand the costs associated with redevelopment (including infrastructure etc). A temporary decant strategy will need to be developed to accommodate existing site operatives during construction. A fully detailed proposal should be developed and the planning department should be fully engaged. If viable, public consultation should begin in close consultation with RBWM.







We have set out below a high level programme from the date of instruction for the delivery of the project should it move forward through to a successful completion on site. This is purely for information purposes and is subject to variation depending on external risk items such as protracted planning negotiations, unfavourable public response, abnormal ground conditions etc.

The below programme is based on the OJEU procurement route however, the iESE framework may create the opportunity to reduce the contractor appointment timescales and reduce the overall programme.

1. Agreement to Proceed
2. Detailed feasibility report and costings – 20 weeks
3. Client sign off
4. Planning design and planning negotiations – 16 weeks
5. Client sign off
6. Planning Determination – 12 weeks
7. Stage E detailed design – 6 weeks
8. Detailed Cost Analysis – 4 weeks
9. Client Sign Off
10. OJEU process – contractor appointment – 36 weeks
11. Contractor Appointment
12. Contractor lead in (including demolition) – 16 weeks
13. Construction period – 24 months

**This programme anticipates approximately 2 years to start on site and 2 years to completion**



For any of the sites included within this report, the successful of this scheme is dependant on the following issues being resolved accordingly during the course of design development:

1. Apportionment of land and construction costs between TVP and RBWM
2. Total resolution of final client brief from TVP & RBWM
3. Determination of detailed surveys including contamination and ground conditions.
4. Decant strategy for existing employees during construction.
5. Positive planning authority support
6. Positive neighbourhood support.
7. Determination of verified budget costings for full quantum of development (including decant costs etc).
8. Material & Labour costs
9. Inflation (BCIS Inflation forecasts show a year on year increase of +6.5% to 2015, +5% to 2016 and +5.5% to 2017)

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## Structural Engineering Report Revision A York House Sheet Street Windsor 11567 For Hunters

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Report For

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Scheme No: 11567

Hunters

York House Sheet Street  
Windsor

Structural Engineering Report  
Revision A

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Main Contributors

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7<sup>th</sup> July 2015

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↘ **Structural Engineering Report Revision A**

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2.0 *Description of Existing Structure*

3.0 *Design Imposed Loads of Existing Floors*

4.0 *Observations in Relation to the Condition of the Existing Structure*

5.0 *Discussion on Feasibility of Proposed Extension Works*

6.0 *Conclusions*

↘ **Appendix A - Existing Survey Plans**

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↘ **Appendix B – Marked Up Survey Plans**

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↘ **Appendix C - Photographs**

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# Structural Engineering Report – Revision A

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## 1.0 Introduction

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- 1.1 This report has been prepared by Tully De'Ath for Hunters who are acting as Architectural Consultant to the Royal Borough of Windsor and Maidenhead (RBWM). This report provides outline structural engineering comments on the feasibility of extending the existing building in height by a single storey and placing a one or two-storey extension on the existing podium deck. This report is to allow RBWM and Hunters to consider the options for the redevelopment of York House in more detail.
- 1.2 York House forms council offices for the RBWM. It is located on the eastern side of Sheet Street in Windsor. The site backs on to parkland and 'The Long Walk' – which forms part of the grounds to Windsor Castle.
- 1.3 The observations and comments made in this report are based on a walk around the building on 25 June 2015 and the following information which has been made available:
- Existing Site Plan, drawing number ASL002 rev A by Hunters;
  - Existing Site Plan, drawing number ASL002 rev A by Hunters marked up to show the 'extension' options that are being considered as part of this engineering report;
  - Survey drawing of Ground Floor Plan, York House, Windsor;
  - Survey drawing of First Floor Plan, York House, Windsor;
  - Survey drawing of rear extension.
- 1.4 During the site visit, most areas of the internal spaces within the undercroft car park level and at ground and first floor levels were visited. Some areas were not accessible due to meetings taking place. Access to the roof space was limited to two small areas which are accessible via one of the main staircases. A step ladder was used to lift ceiling panels locally in a couple of areas, otherwise no opening up works or other investigations have taken place
- 1.5 Existing survey plans of the building are enclosed in Appendix A.
- 1.6 Marked up copies of the survey plans showing possible column positions are enclosed within appendix B.
- 1.7 Photographs of the building referred to in the report are enclosed in appendix C.
- 1.8 Revision A of this report has been prepared following comments received from Hunters and RBWM.





## 2.0 Description of Existing Structure

- 2.1 York House consists of a number of inter-linked two storey blocks which sit on a podium deck above an undercroft car park. These blocks are laid out in an inverted 'U' shape with the front block facing west on to Sheet Street, the middle section running beside the northern boundary of the site, and the rear block placed at the eastern end of the building beside 'The Long Walk'. The building is brick-clad and has pitched roofs which are tiled. See photographs 1 to 5 for external images of the building.
- 2.2 The undercroft car park is accessed via a road to the south of the building. This drops down from street level off Sheet Street to the undercroft car park entrance which is positioned centrally along the south side of the podium deck (see photograph 3). The podium deck covers the remaining area of the car park not covered by buildings (see photographs 4 and 5). This is paved with concrete pavers and has brick-clad parapet walls around the perimeter. The podium area accommodates two lightwells (see photograph 6) and has a lightweight steel and glass covered walkway providing pedestrian links between the blocks
- 2.3 From discussions with the facilities manager during the site visit, it is understood the building dates from the 1970's. It was later extended in the 1990's when the rear two-storey block was added

### The Original Structure

- 2.4 The existing structure of the original building is not entirely clear but there are clues from observations made during the site visit and from a review of the survey drawings. This review suggests the original building appears to be a reinforced concrete frame supporting a first floor concrete slab and a timber trussed rafter roof. This is supported off a ground floor transfer structure and podium deck over the car park area. The weight of the building is transferred to the ground via exposed columns within the car park.
- 2.5 Potential column positions are implied on the survey plans both within the internal spaces and within the elevations of the building (see photograph 9). The positions of these are shown on marked up survey plans within appendix B.
- 2.6 The ground floor transfer structure and podium deck are formed using an in situ reinforced concrete waffle slab with beam strips linking between columns (see photographs 7 and 8). Where ceiling panels were lifted internally, the original first floor structure was also noted to be a concrete waffle slab.
- 2.7 It is not clear how the stability of the original building is achieved. Usually concrete shear walls are used and these may exist around the lift shaft and within sections of the elevations. This needs to be confirmed.
- 2.8 The internal spaces are laid out either as open plan areas or as offices and meeting rooms (see photograph 9). Some of the internal walls are brick-faced and some plastered walls feel solid when tapped. The accessible area of the roof space is used for storage of files (see photograph 10).
- 2.9 The brickwork to the elevations appears to be non-structural and takes support off the podium deck. It was noted there are very few control joints in the masonry.

### The Rear Extension

- 2.10 The rear extension sits at the eastern end of the building. This appears to have been added on top of the podium deck which originally appears to have been built to the rear of the building. Two original lightwells appear to have been infilled with an in situ concrete slab to form part of the ground floor to this extension (see photograph 8).



- 
- 2.11 This rear extension is steel framed with steel columns supporting a first floor structure and roof structure. The first floor has profiled metal decking supporting a concrete slab off steel beams. The roof structure was not seen but is probably a timber trussed rafter roof supported off steel beams
  - 2.12 The positions of the columns are shown on marked up survey plans within appendix B
  - 2.13 The internal spaces are laid out either as open plan areas or as offices and meeting rooms
  - 2.14 It is not clear how the stability of the rear extension is achieved. Usually such building structures have steel cross bracing in braced bays and these are normally placed around stairwells and along elevations where there are no door or window openings. This needs to be confirmed.
  - 2.15 The brickwork to the elevations appears to be non-structural and takes support off the podium deck.

#### Ground Conditions and foundations

- 2.16 The local geology map indicates the natural ground conditions on the site consist of Head Deposits overlying the Lambeth Group. The geology map describes the Head deposits to consist of clay, silt, sand and gravel in the area around the site, and the Lambeth Group to consist of clays, silts and sands
- 2.17 If the ground conditions on the site are reasonable and there is a limited depth of made ground, the existing foundations would be expected to be reinforced concrete pad footings. If the ground has a limited bearing capacity or there is a significant depth of made ground, piled foundations are more likely. The existing foundations have not been confirmed however



### 3.0 Design Imposed Loads of existing floors

3.1 The codes of practice that were relevant at the time of the building design have been reviewed to give an indication of what the floors could have been designed for, with respect to imposed loads.

3.2 The relevant codes are as follows:

For the original building (assumed to date from the 1970's)	British Standard Code of Practice CP3: Chapter V: Part 1: 1967: Code of Basic Data for the design of buildings – Chapter V Loading: Part 1 Dead and Imposed Loads.
For the rear extension (assumed to date from the 1990's)	BS 6399: Part 1: 1984 British Standard Loading for Buildings Part 1. Code of Practice for dead and imposed loads.

3.3 These codes indicate the minimum recommended imposed loads for office use is as follows:

	Imposed loading in kN/m <sup>2</sup>	
	Original Building c.1970's	Rear Extension c.1990's
Offices for general use	2.5	2.5
Toilet areas	2.0	2.0
Filing and storage spaces	5.0	5.0
Terraces (podium deck?)	4.0	
Corridors, hallways, stairs	2.5	4.0

3.4 In addition to these loads, both codes of practice recommend a minimum allowance for lightweight partitions of at least 1.0 kN/m<sup>2</sup>. Where masonry walls are proposed, this loading allowance would need to be increased.

3.5 This shows that most floors for general office use were designed for a minimum imposed load of 2.5kN/m<sup>2</sup> plus at least 1.0kN/m<sup>2</sup> for partitioning. Office floors however were often designed for 4.0kN/m<sup>2</sup> plus at least 1.0kN/m<sup>2</sup> to provide more flexibility on how the floors were used. Such an approach allowed for some filing to be stored on the office floors too. Given York House contains a number of masonry walls, a higher partition loading allowance is likely to have been used.

3.6 The actual imposed loads the floors were designed for would need to be confirmed via a review of the original structural engineering drawings or calculations.



## 4.0 Observations in relation to the condition of the existing structure

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- 4.1 From a walk around the internal and external areas of the building, the building appears to be in a reasonable condition for its age and type.



## 5.0 Discussion on feasibility of proposed extension

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- 5.1 The proposals being considered are to introduce a single storey extension on top of the existing two-storey blocks and a one or two storey block on the existing podium deck. Each proposal is discussed in turn below.

### Proposal to Add a Single Storey Extension on Top of the Existing Blocks

- 5.2 This proposal involves removing the existing pitched tiled roofs and replacing them with a lightweight steel frame. Both residential and office use are being considered for this extension, which will be set back slightly from the existing elevations.
- 5.3 Given the anticipated form of the existing structure, the new extension would need to be designed so that it is supported directly off the existing column positions. The extension would therefore require a transfer structure formed from a grillage of steel beams to support the new set-back extension off the existing columns. If office use is being considered, it is assumed the space will be for general office use with no areas of heavy storage and partitioning would be lightweight and limited in its extent. The elevations are also assumed to be of lightweight construction. The comments which follow are based on this approach.
- 5.4 The weight of the pitched roofs to be removed will be less than the weight of the new extension. The increased loading on the structure therefore needs to be analysed in more detail to see if it is justified.
- 5.5 For concrete framed structures, columns tend to increase in strength, over a number of years, beyond what they were designed for. This is related to the curing of the concrete. A structural design is normally based on a target concrete strength once it is 28 days old. This tends to equate to about 90% of the overall strength of the concrete that it eventually achieves. Based on the assumed column sizes suggested by the survey drawings and at basement level, and an assumed concrete design strength, the further 10% of concrete strength (that has developed since 28 days old) would be sufficient to support the increased loads from a new lightweight extension. This would need to be checked through opening up works and concrete testing.
- 5.6 There is no similar increase in strength with steel columns, which exist in the rear extension. Opening up works would need to be carried out to check the column sizes so that they can be back-analysed. If they have inadequate strength, there are options available to strengthen them, such as bolting steel angles or channels to the existing columns. If existing structural record drawings could be found, this could reduce the amount of opening up works required.
- 5.7 It appears the podium deck acts as a transfer structure as the columns above the podium deck do not appear to align with the columns at car park level. If this is the case, the existing transfer structure will need to support the additional loads from the new extension. The load increase would be in the order of 11%. Without details of the transfer structure, this increase cannot be justified. It is important to realise here that the capacity of the slab is likely to be more directly linked with the reinforcement within the slab rather than the strength of the concrete. A measured survey of the undercroft is needed so that the column positions above and below can be related to one another.



- 5.8 An alternative way to justify the transfer structure would be to examine whether the floors were designed for an imposed load of  $4.0\text{kN/m}^2$ . If filing storage is not necessary in the office areas, an office loading of  $2.5\text{kN/m}^2$  should be sufficient for the future use of the building. If the floors were originally designed for  $4.0\text{kN/m}^2$ , then the spare capacity would be sufficient to justify the additional load from the single storey extension. To prove the floors in this way would be very disruptive in terms of opening up works. If record drawings and original calculations are available, this would provide much more certainty and reduce the level of opening up works required.
- 5.9 Building Control normally accept a 10% increase in loads on existing foundations where the building has performed satisfactorily for a number of years. The new extension would generate increased loads of about this order. Such an approach would need to be agreed with Building Control. If record drawings of the as built structure were available this would allow the analysis to be far more certain. It would however be prudent to carry out some investigations, once – and if - the podium deck can be demonstrated to support the increased loads from the new extension.
- 5.10 Although mainly non-structural, using the new extension for residential use is likely to be more onerous than office use. Separate access and means of escape would be required and the acoustic and fire compartmentation aspects may be more challenging. Residential use also requires more servicing and integrating services into the structure. An example of this would be svp's internally within the extension which would need to drop and then run horizontally above the existing first floor spaces.
- 5.11 As the existing roof is formed using timber trussed rafters, this will need to be removed in its entirety before a new second floor structure can be added. This will mean the first floor areas currently in use would need to be taken out of use for a period of time during the works.

#### Proposal to Add a Single or Two Storey Extension on the Existing Podium Deck

- 5.12 This proposal involves the introduction of a new one or two storey extension. This is to be used for office space or as a police station so there will be areas where the general public can visit and areas for office use. The area shown measures approximately 10m by 15m.
- 5.13 It is assumed the new extension would need to be designed so that it is supported on the existing column positions. Where new and existing columns therefore do not align, a new transfer structure formed from a grillage of steel beams set above the podium deck will be required. It is assumed the office space will be for general office use with no areas of heavy storage and partitioning would be lightweight and limited in its extent. The elevations are also assumed to be of lightweight construction. The comments which follow are based on this approach.
- 5.14 The columns within the undercroft appear to be of a consistent size across most areas of the building. It is therefore considered likely that the existing columns will be adequate in strength to support the weight of a new two storey extension, provided the concrete design strength was reasonable. The increase in weight on these columns however will exceed 10%. Concrete testing and intrusive surveys would be required to check the make up of the existing columns which are affected by this. Trial pits would also be necessary to check the existing foundations.
- 5.15 As with the roof top extension, record drawings of the existing structure and original design calculations would provide far more certainty as to the limitations of the existing structure, and this may allow the extent of investigations required to be reduced.
- 5.16 A survey of the undercroft columns is required to confirm how the grillage of steel beams will be arranged. This may indicate the building should take a slightly different shape to that which is currently indicated. Such an exercise will also allow the depth of the steel grillage to be determined so that this can be incorporated into the architectural proposals moving forward.
- 5.17 The new extension will require one of the existing lightwells in the podium deck to be infilled. It is likely that this can be done in a similar way to the infill that was carried out when the 1990's extension was added.



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## 6.0 Conclusions

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- 6.1 The original podium deck structure and the majority of the office blocks forming York House appear to date from the 1970's. A further block exists on the podium deck at the rear and this appears to have been added in the 1990's.
- 6.2 The original structure of the building is not very clear from observations on site but survey drawings suggest it is probably a reinforced concrete framed structure. Record structural drawings or opening up works are required to confirm this. The rear extension is a steel framed building.
- 6.3 The new roof top extension will be heavier than the weight of the pitched roofs that would be removed. The existing structure would therefore need to be justified for the increased loads. The transfer structure forming the podium deck is the biggest concern to this increase in load as it cannot be justified based on the limited level of information currently available.
- 6.4 Record drawings and design calculations of the structure are needed to clarify how the structure was designed. If not, significant opening up works and concrete testing would be required to justify aspects of the structure to support the increase in loads.
- 6.5 A measured survey of the basement is needed. This will assist with the structural appraisal of the podium deck to act as a transfer structure and to show how columns above and below this level relate to one another.
- 6.6 The extension of the podium deck should be achievable – but this is subject to further surveys, opening up works and concrete testing. Some strengthening works may be required.
- 6.7 The extension to the roof top appears more onerous. If record drawings and calculations of the structure are not available there would be a need for further surveys, opening up works and concrete testing. This may show the proposal is viable or may show it is not without some form of strengthening works. Such strengthening works could be very disruptive – such as involving new columns within the basement in locations which would be awkward to how the car park functions.



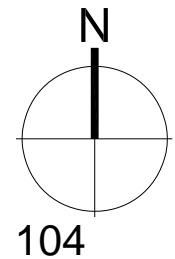
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## Appendix A – Existing Survey Plans

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Revision			
A	25.06.14	mtb	legal boundary amended in accordance with title no. SU9676se



project name: **TVP & RBWM Combined Office**  
drawing reference: **York House, Sheet Street Existing Site Plan**  
job number: **M8928**  
status: **PRELIMINARY**  
cad ref: **York House Sheet Street Existing Site Plan.dwg**

date: **24.06.14**  
sheet: **1:500**  
scale: **1:500**

drawing number: **ASL002**  
revision: **A**

drawn: **mtb**  
checked: **XXX**

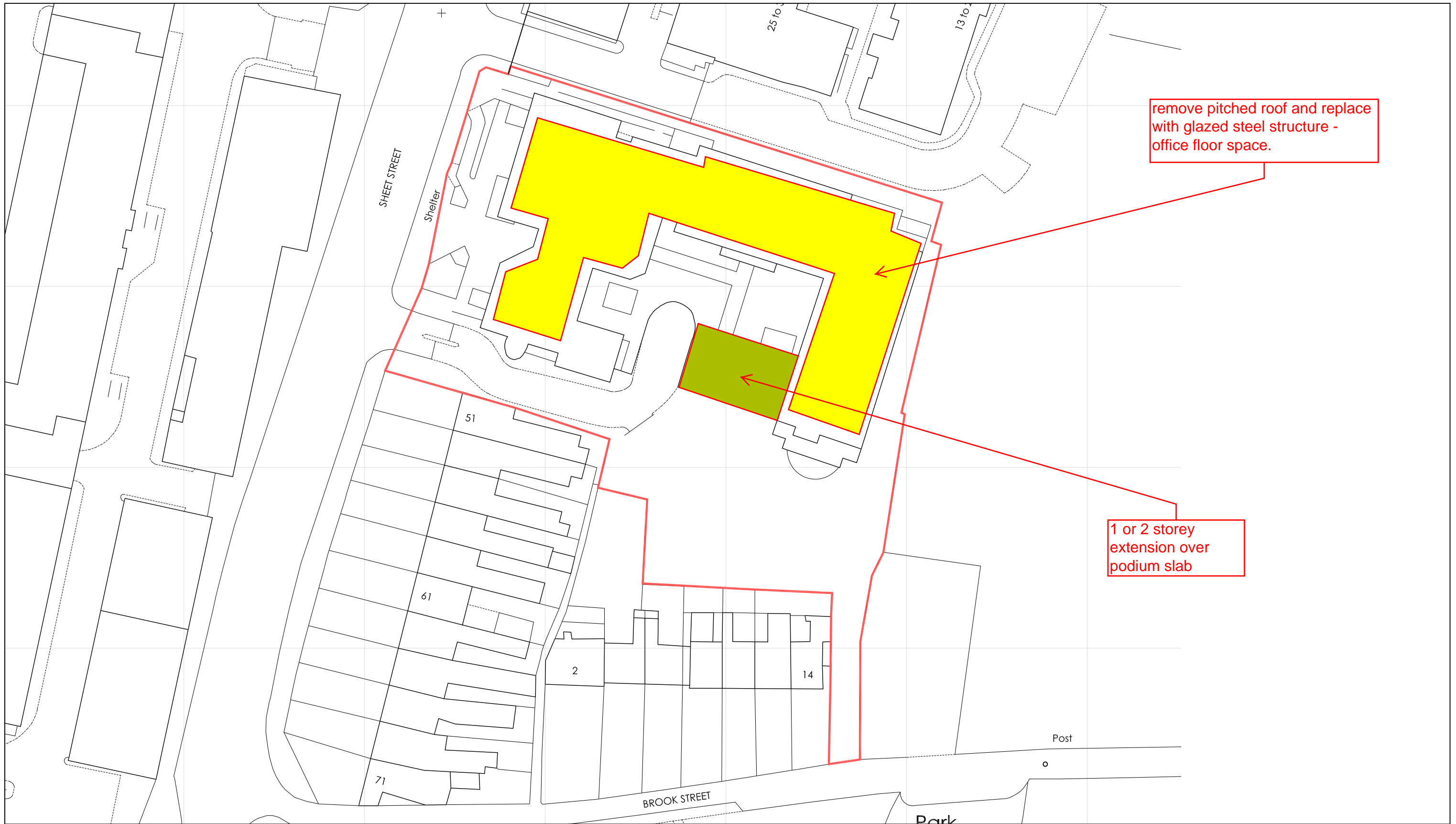
Use figured dimensions only. All levels and dimensions to be checked on site. This drawing is to be read in conjunction with all other relevant drawings and specifications. Hunters is a trading name of Hunter & Partners Limited. © Hunter & Partners Limited. All rights reserved.

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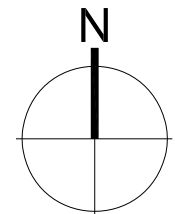
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Revision			
A	25.06.14	mtb	legal boundary amended in accordance with title no. SU9676se



project name: **TVP & RBWM Combined Office**  
drawing reference: **York House, Sheet Street Existing Site Plan**  
date: **24.06.14**  
sheet: **1:500**  
scale: **1:500**  
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revision: **A**  
status: **PRELIMINARY**  
cad ref: **York House Sheet Street Existing Site Plan.dwg**

drawn: **mtb**  
checked: **XXX**

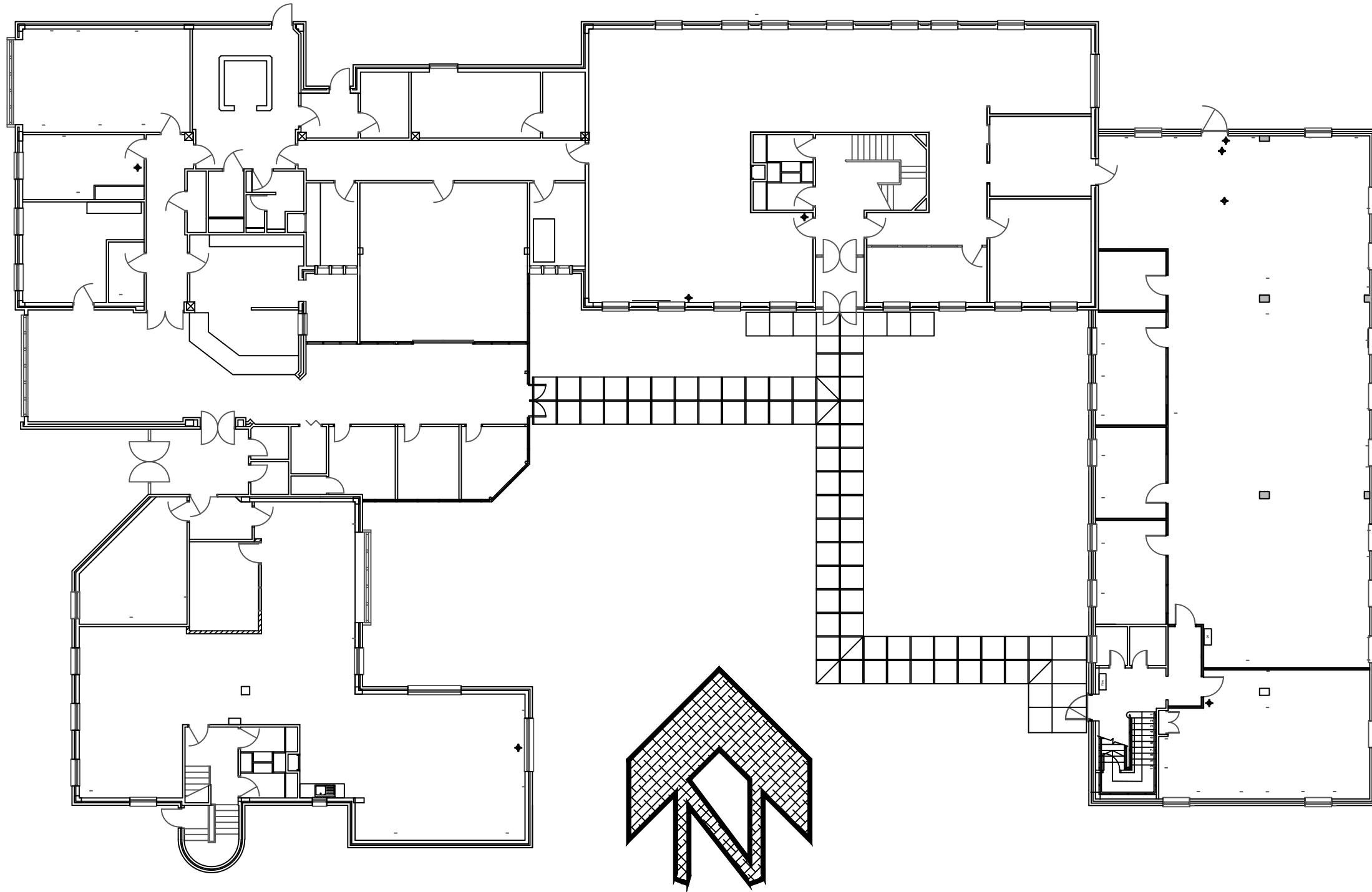
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# Ground Floor, York House, Windsor.

Scale 1:200 @ A3

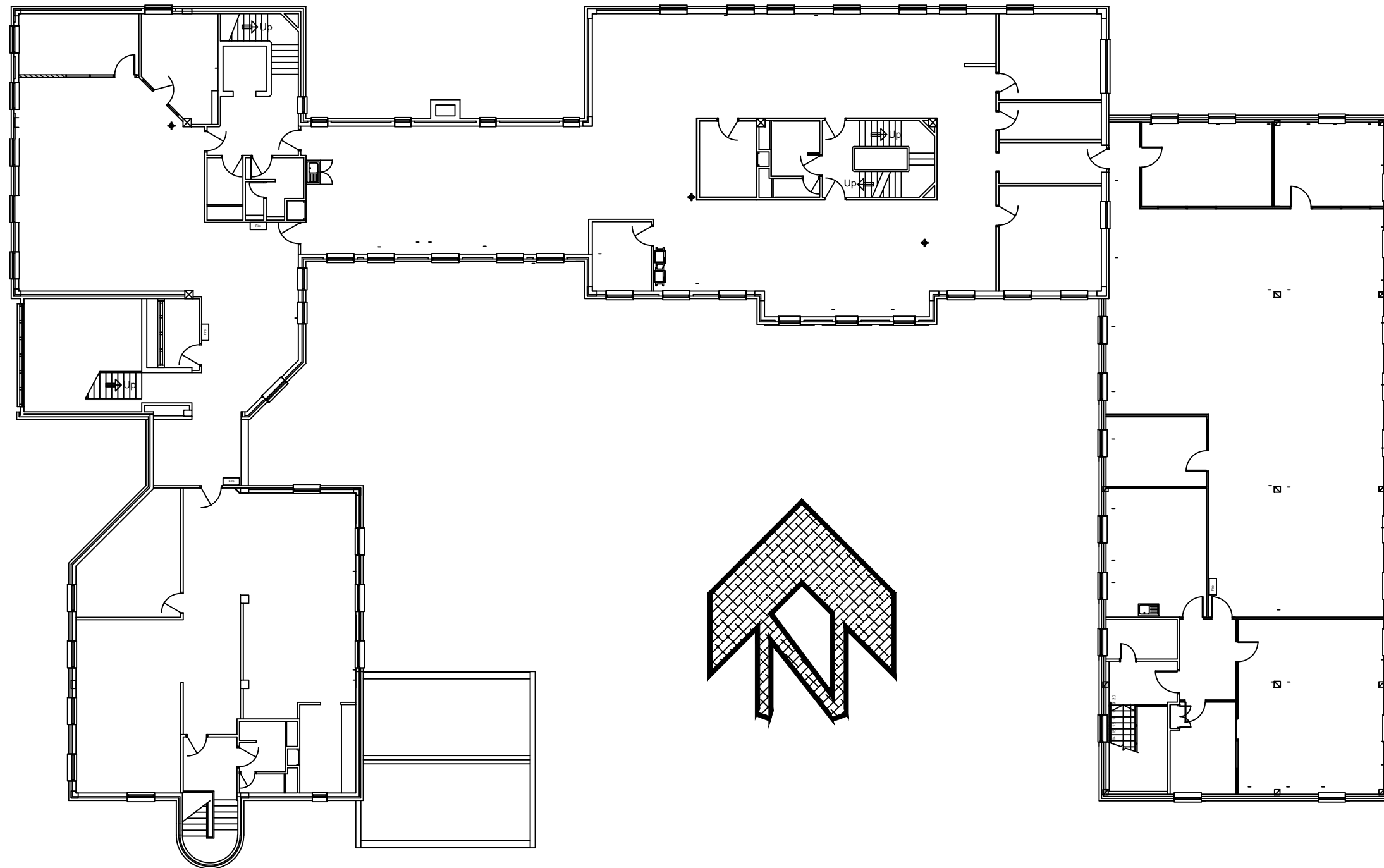
(Sheet 1 of 2)



# First Floor, York House, Windsor.

Scale 1:200 @ A3

(Sheet 2 of 2)





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## Appendix B – Marked Up Survey Plans

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# Ground Floor, York House, Windsor.

● DENOTES ASSUMED COLUMN POSITION IN ORIGINAL BUILDING

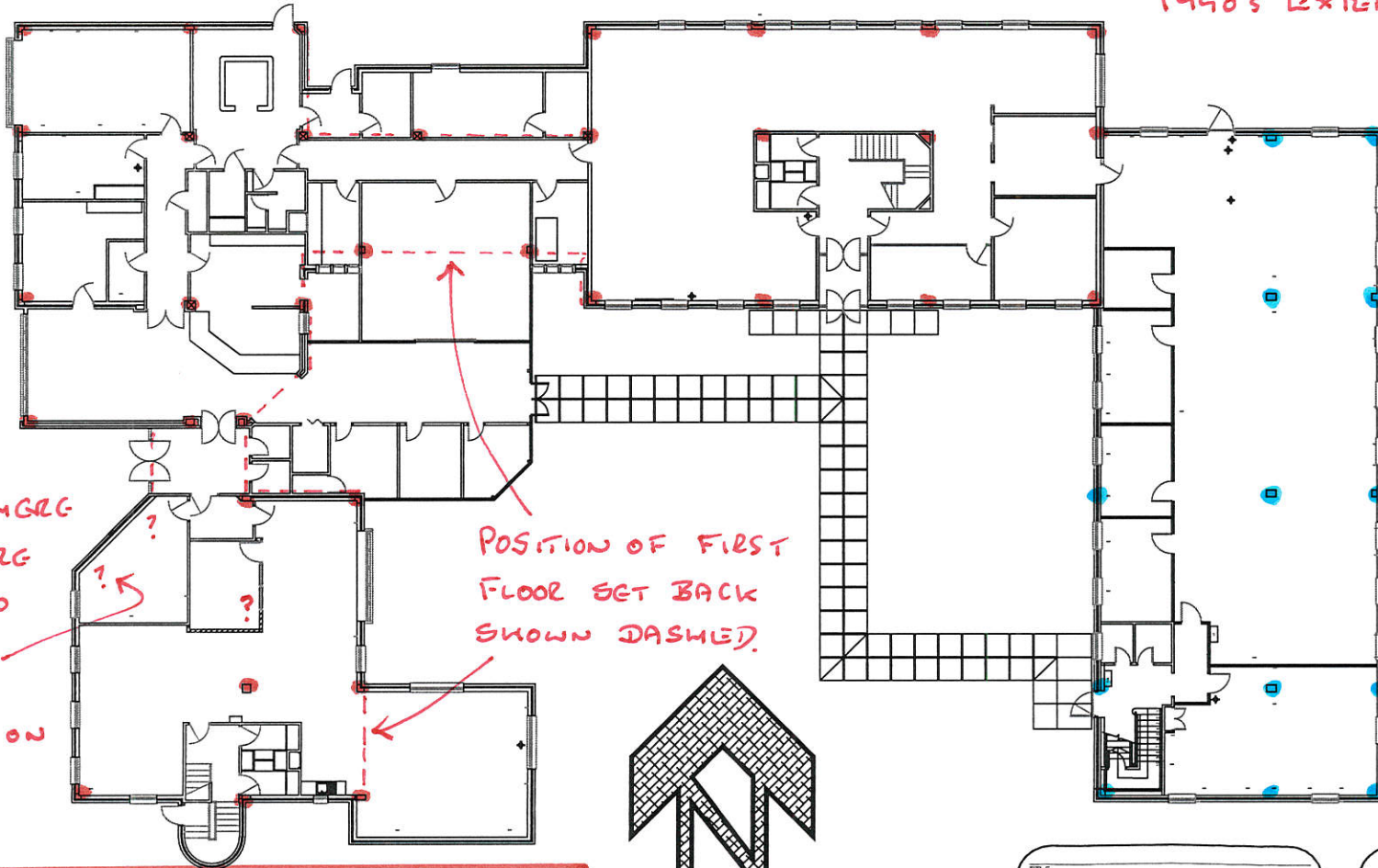
Scale 1:200 @ A3  
(Sheet 1 of 2)

● DENOTES ASSUMED COLUMN POSITION IN 1990'S EXTENSION.

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? DENOTES POSITIONS WHERE COLUMNS ARE EXPECTED TO EXIST BUT ARE NOT SUGGESTED ON SURVEY DRAWINGS

POSITION OF FIRST FLOOR SET BACK SHOWN DASHED.



COLUMN POSITIONS ASSUMED ARE BASED ON REVIEW OF RECORD SURVEY DRAWINGS.

FILE: ASSUMED COLUMN POSITIONS - GROUND  
PROJECT: YORK HOUSE WINDSOR  
SCALE: DATE: DRAWN: CHK'D:  
JOB NO. DPC NO. REV  
11567 SK01

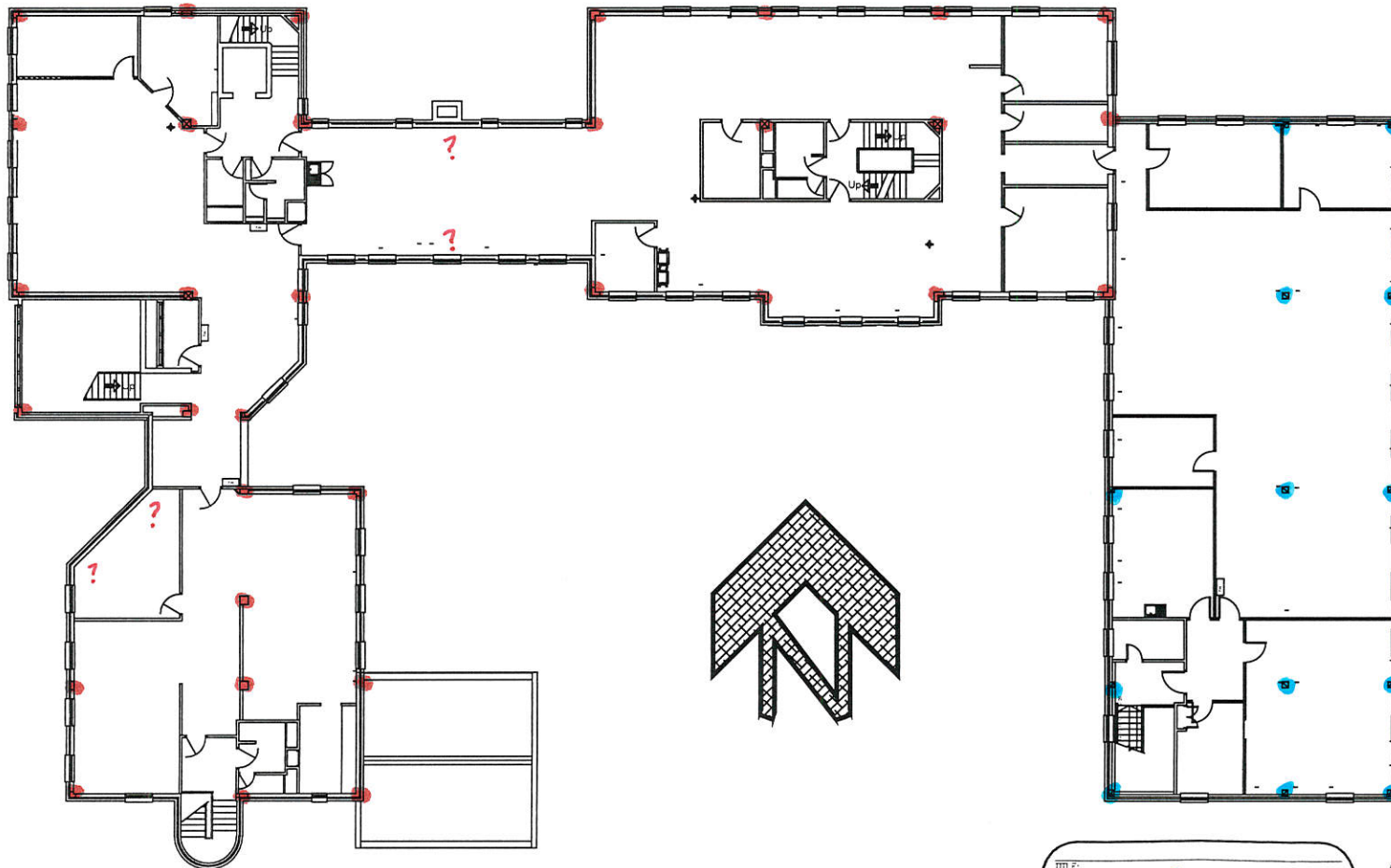
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# First Floor, York House, Windsor.

Scale 1:200 @ A3  
(Sheet 2 of 2)

REFER TO GROUND FLOOR  
PLAN FOR KEY TO NOTES.

110



TITLE: ASSUMED COLUMN POSITIONS - FIRST  
PROJECT: YORK HOUSE WINDSOR

SCALE:	DATE:	DRAWN:	CHK'D:
JOB NO.	DRG NO.	REV.	
11567	5102		

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EMAIL info@tullydeath.com WEB SITE www.tullydeath.com



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## Appendix C – Photographs

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**Photograph 1**



**Photograph 2**





**Photograph 3**



**Photograph 4**



Photograph 5



Photograph 6



Photograph 7



Photograph 8



**Photograph 9**



**Photograph 10**

## **Feasibility** Research

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